



WASHINGTON UTILITIES AND TRANSPORTATION COMMISSION

Washington State Department of Transportation

Petitioner,

vs. *KET LLC*

d.b.a. Kennewick Terminal Railway aka ~~KET LLC~~
Respondent

DOCKET NO. TR- *200074*

PETITION TO MODIFY WARNING DEVICES AT A HIGHWAY-RAILROAD GRADE CROSSING

USDOT: 808925X

State Of WASH.
UTIL. AND TRANSP.
COMMISSION

02/18/20 08:52

Received
Records Management

The Petitioner asks the Washington Utilities and Transportation Commission to approve modification of warning devices at a highway-rail grade crossing.

Section 1 – Petitioner’s Information

Washington State Department of Transportation
Petitioner
Signature
310 Maple Park Avenue, SE, SUITE 2B
Street Address
Olympia, WA 98504
City, State and Zip Code
PO Box 47329, Olympia WA 98504-7329
Mailing Address, if different than the street address
Connie Raezer
Contact Person Name & Signature
360-705-7459 or raezerc@wsdot.wa.gov
Contact Phone Number and Email Address

Section 2 – Respondent's Information

KET LLC
Respondent
709 North 10 th Avenue
Street Address
Walla Walla, WA 99362
City, State and Zip Code
Mailing Address, if different than the street address
Paul Didelius – Owner/President
Contact Person Name
509-492-3553 or pd@columbiarail.com
Contact Phone Number and Email Address

Section 3 – Crossing Location

1. Existing highway/roadway	State Route 397
2. Existing railroad	KET Railroad LLC d.b.a. Kennewick Term. RR
3. USDOT Crossing No.	808925X
4. GPS location	46.2106600 -119.1071200
5. Railroad mile post (nearest tenth)	12.830 N.A.
6. City	Kennewick
	County Benton

Section 4 – Vehicle Traffic

1. Name of highway State Route 397

2. Road authority Washington State Department of Transportation

3. Average annual daily traffic (AADT) 16,000 (2016)

4. Number of lanes 5

5. Roadway speed 35 mph

6. Is the crossing part of an established truck route? Yes X No

7. If so, trucks are what percent of total daily traffic? 6%

8. Is the crossing part of an established school bus route? Yes X No

9. If so, how many school buses travel over the crossing each day? 10 (estimated)

10. Describe any changes to the information in 1 through 7, above, expected within ten years:
Local development of housing and businesses is expected to increase vehicle traffic counts.

Section 5 – Current Crossing Information

Kennewick Terminal

1. Railroad company ~~KEF~~ Railroad (UP is still showing on FRA report, line was sold)

2. Type of railroad at crossing Common Carrier Logging ~~Industrial~~
 Passenger Excursion

3. Type of tracks at crossing Main Line Siding or Spur

4. Number of tracks at crossing 1

5. Average daily train traffic, freight less than one a day 6 a week at last report
Authorized freight train speed 10 mph Operated freight train speed 6-7

6. Average daily train traffic, passenger n/a
Authorized passenger train speed n/a Operated passenger train speed n/a

7. Describe any changes to the information in 1 through 4, above, expected within ten years:

No known

8. What is the available sight distance from the stop bar (or 25 feet from the tracks if no stop bar) on both approaches to the crossing?

NB on 397 = More than 300 feet to the east and more than 300 feet to the west

SB on 397 = More than 300 feet to the west and more than 200 feet to the east

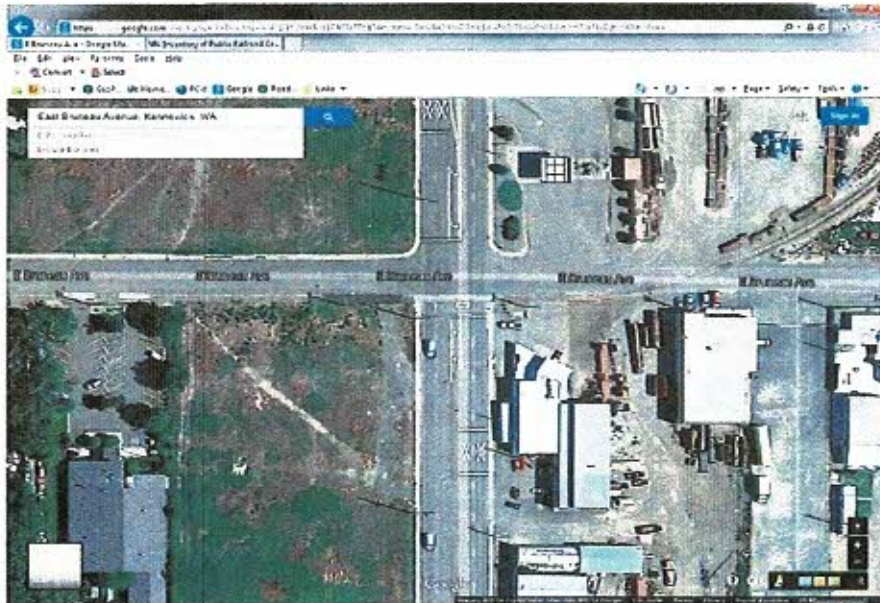
9. If the sight distance is less than 400 feet, describe the structures, roadway or track curvature, visual obstacles or other characteristics that limit sight distance.

There is some brush in the way, track curvature into industrial area on the east side of crossing

Section 6 – Current Warning Devices

Provide a complete description of the warning devices currently located at the crossing (vehicle and pedestrian), including signs, gates, lights, train detection circuitry and any other warning devices.

This is currently a passive crossing, with advance warning signs (W10-01) and stop bars as well as appropriate pavement markings.



Section 7 – Description of Proposed Changes

Describe in detail the number and type of proposed automatic signals (vehicle and pedestrian), gates or other warning devices, and/or changes to train detection circuitry. Please describe any other proposed changes at the crossing, including changes to the crossing surface, signage, pavement markings, etc. If sidewalks are being installed, please provide information on who will maintain them. (Attach additional information sheets, if needed.)

Install Train detection island circuit, which will require all train movements to stop and proceed after signals activate. Install new track and crossing surface as required to support track circuitry. Install Cantilever-mounted signals that extends to at least the center of the innermost (left turn) lane. One set of sidelight flashers on SE quadrant will be directed west towards castbound Bruneau traffic (mounted on tip of SE cantilever), and one set of sidelight flashers directed east towards westbound Bruneau traffic. Install signal cabinet/house. Modify sidewalk to direct pedestrians behind cantilever foundation on NW quadrant. Install updated signs on all approaches.

Improvements will be funded under Section 130.

Railroad will maintain rail infrastructure and road authority will maintain, sidewalks, pavement markings and signage as prescribed in law.

Section 8 – Illustration of Proposed Warning Devices

Attach a detailed design diagram, drawing, map or other illustration showing all proposed modifications, including signals, signage, pavement markings, sidewalks, etc.

See attached plans prepared by KET Railroad

Section 9 – Waiver of Hearing by Respondent

Waiver of Hearing

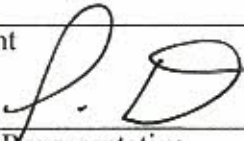
The undersigned represents the Respondent in the petition to modify highway-rail grade crossing warning signal system at the following crossing.

USDOT Crossing No. 808295X

We have investigated the conditions at the crossing. We are satisfied the conditions are the same as described by the Petitioner in this docket. We agree the warning signal system should be modified and consent to a decision by the commission without a hearing.

Dated at Walla Walla, Washington, on the 14th day of February, 2020.

Paul Didelius
Printed name of Respondent


Signature of Respondent's Representative

Owner/President
Title

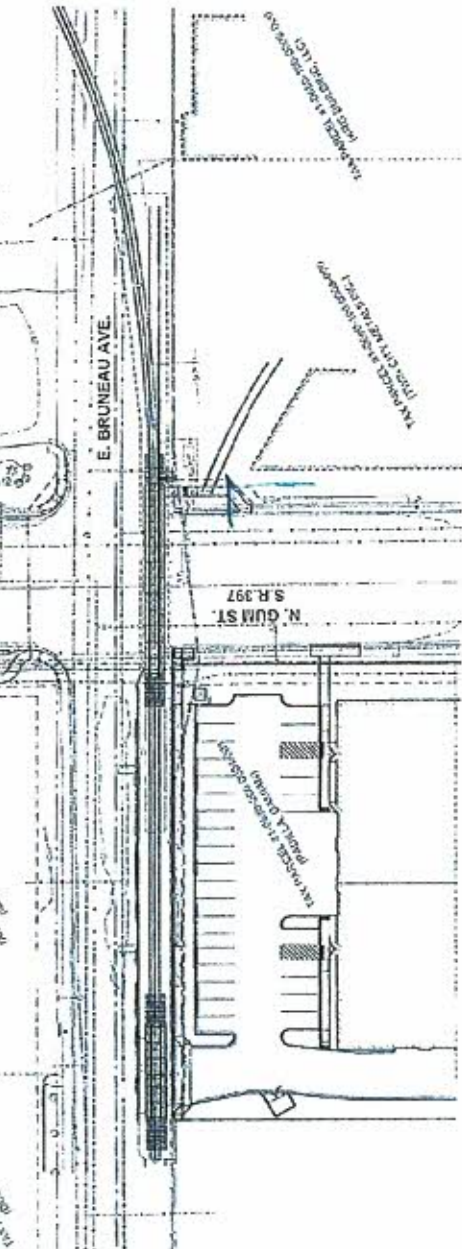
509-492-3553 or pd@columbiarail.com
Phone number and e-mail address

709 North 10th Avenue

Walla Walla, WA 99362
Mailing address

BRUNEAU ROAD CROSSING

LOCATED IN A PORTION OF THE NORTHERN 1/2 OF SECTION 6, TOWNSHIP 8 NORTH, RANGE 30 EAST, W.M.



COVER SHEET AND GENERAL NOTES
KENNEBEC, WA
BRUNEAU ROAD CROSSING

KENNEBEC TERMINAL RAILROAD
425 SE 3RD AVE., SUITE 205
PORTLAND, OR 97214
(503) 888-0011

CONTRACTOR OFFICE
PACIFIC RAILROAD
433 SE 3RD AVE.
SUITE 205
PORTLAND, OR 97214
(503) 888-0011

ENGINEER
TOMAS MOSEK, PE
2790 SW BEAUMONT DR
TUALATIN, OR 97062
(503) 891-1099

SURVEYOR
ALEX MATHIASZKO, PLS
400 BRADLEY BLVD.
SUITE 201
RICHLAND, WA 99352
(509) 843-1000

SHEET INDEX

61 COVER SHEET AND GENERAL NOTES
62 CIVIL SITE AND UTILITY PLAN
63 PROFILE
64 NORTH CANTILEVER
65 SOUTH CANTILEVER

PRELIMINARY
SUBJECT TO PERMITS
NOT FOR CONSTRUCTION

BENTON PUD
Approved by Project #22 Date _____

FRONTIER
Approved by Project # Date _____

CHARTER COMMUNICATIONS
Approved by Charter License/Agency _____

CITY OF KENNEBEC
Approved by City of Kennebec Public Works Date _____

CASCADE NATURAL GAS
Approved by Utility Number 001 _____ Date _____

Water _____
Sanitary Sewer _____
Storm _____

GENERAL NOTES:

BASE OF ELEVATIONS CITY OF KENNEBEC BENCHMARK 0812 3.47 BRASS CAP IN MONUMENT CASE, H.C.C. CITY OF KENNEBEC SURVEY ELEVATION OF 36.07 PER SURVEY IN BOOK 1 OF SURVEYS AT PAGE 194.

THE CONTRACTOR SHALL SCHEDULE A PRECONSTRUCTION MEETING WITH THE CITY ENGINEER, (509) 944-4347, AT LEAST 2 WEEKS PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITY.

48 HOUR NOTICE TO THE CITY OF KENNEBEC, (509) 944-4347, IS REQUIRED PRIOR TO ANY ACTIVITY WITHIN THE CITY 500 FOOTWAY OR ANY ACTIVITY INVOLVING CITY SWER STORM OR WATER.

A MINIMUM OF TWO BUSINESS DAYS PRIOR TO BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL CALL 811 UTILITY LOCATE FOR LOCATION MARKUP OF EXISTING UTILITIES.

CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL BARRIERS IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AS REQUIRED PRIOR TO DEPARTURE OF ANY TRAFFIC. A TRAFFIC PLAN SHALL BE PREPARED AND SUBMITTED TO THE CITY ENGINEER FOR APPROVAL. NO WORK SHALL COMMENCE UNTIL ALL APPROVED TRAFFIC CONTROL IS IN PLACE.

AT THE END OF EACH DAY, THE CONTRACTOR SHALL BE RESPONSIBLE FOR RETURNING THE ENTIRE WIDTH OF THE TRAVEL ROADWAY BACK TO COMPLETE VEHICULAR TRAVEL. THIS CONTRACTOR SHALL BE LIMITED TO ALL TRUCKS BACK FILLED ON COMBINED WITH TRUCKS. ALL TRUCKS SHALL BE FILLED ON COMBINED WITH TRUCKS. ALL TRUCKS SHALL BE FILLED ON COMBINED WITH TRUCKS.

ALL EQUIPMENT, SUPPLIES, AND MISCELLANEOUS ITEMS REMOVED FROM THE RIGHT-OF-WAY.

EROSION, SEDIMENT, AND AIR QUALITY CONTROL SHALL COMPLY WITH BENTON COUNTY EROSION CONTROL ORDINANCES, AND THE NOTES AND DETAILS ON THESE PLANS.

REINFORCEMENT WITHIN THE PROJECT WILL COMPLY WITH THE BENTON COUNTY DEPARTMENT OF LABOR AND INDUSTRIES SAFETY STANDARDS FOR EXCAVATION, TRENCHING, AND SHORING (WAC 296-155-060 THRU WAC 296-155-061).

SITE GRADING, STREET, STORM SEWER, SANITARY SEWER, AND POTABLE WATER SYSTEMS SHALL COMPLY WITH THE BENTON COUNTY SPECIFICATIONS FOR ROAD, BRIDGE AND MUNICIPAL CONSTRUCTION (M-9) AND THE CONSTRUCTION STANDARDS OF THE CITY OF KENNEBEC PUBLIC WORKS DEPARTMENT.

ALL ELECTRICAL SYSTEMS, STREETLIGHTS, AND ELECTRICAL CROSSING LOCATION CONSTRUCTION MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE LATEST STANDARDS AND PRACTICES OF BENTON COUNTY PUBLIC UTILITY DISTRICT.

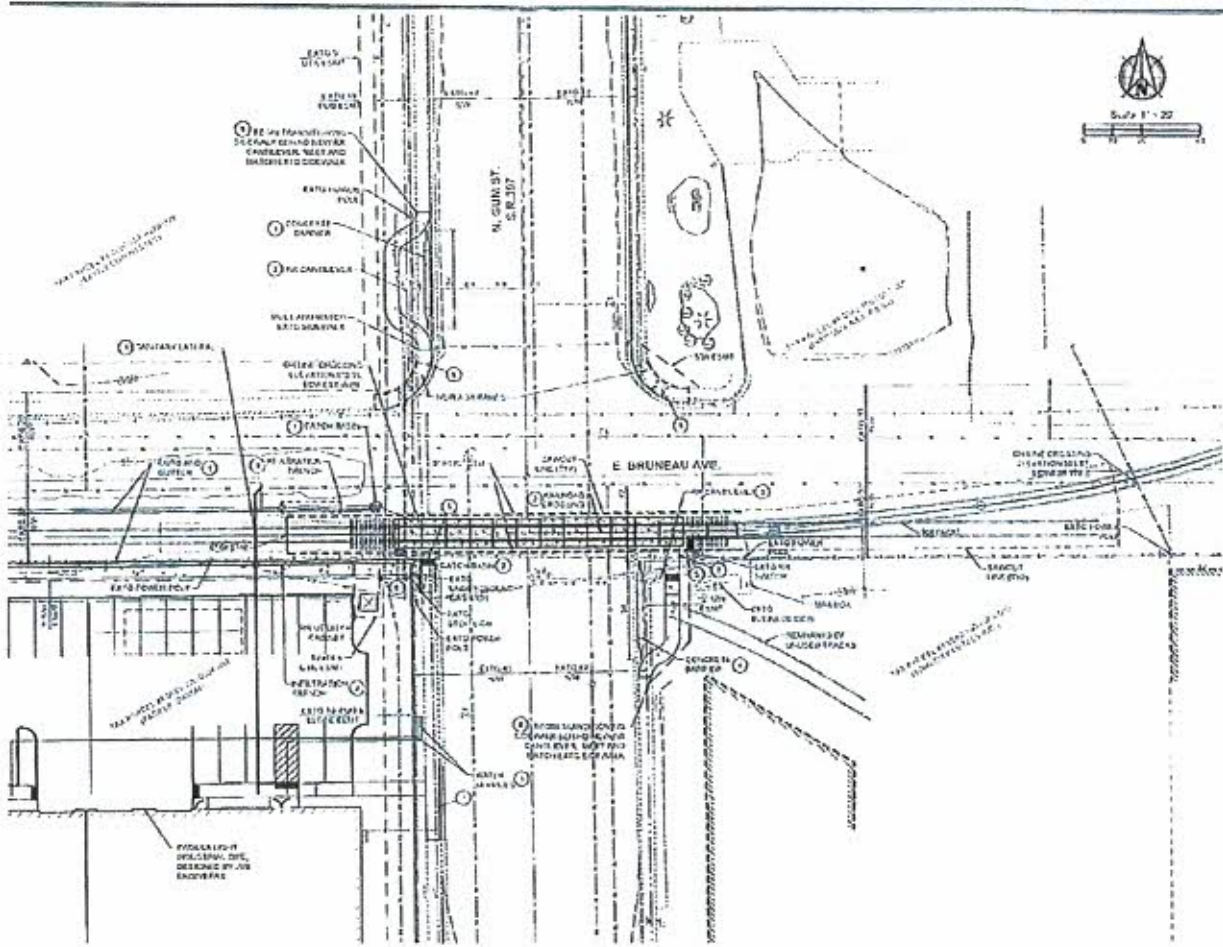
ALL IRRIGATION SYSTEMS, IRRIGATION MAINS, AND IRRIGATION SERVICE CROSSING LOCATION CONSTRUCTION MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE LATEST STANDARDS AND PRACTICES OF KENNEBEC IRRIGATION DISTRICT.

THE CONTRACTOR SHALL AT ALL TIMES OBSERVE AND COMPLY WITH ALL FEDERAL, STATE, AND LOCAL LAWS, ORDINANCES, AND REGULATIONS WHICH IN ANY MANNER AFFECT THE CONDUCT OR SAFETY OF THE WORK.

CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO BEGINNING CONSTRUCTION.

EXISTING UTILITY LOCATIONS ARE APPROXIMATE AND MUST BE VERIFIED BY THE CONTRACTOR. OTHER UTILITIES MAY EXIST. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL LOCATE AND MARK EXISTING UTILITY LOCATIONS AND ELEVATIONS WHERE INTERFERENCE WITH THEM IS NOT NEARLY AVOIDABLE.





WRE
WESTERN RAIL ENGINEERING
 William W. Wilkerson, P.E.
 1111 1/2 4th St.
 Astoria, OR 97103
 Phone: 503-325-3333
 Fax: 503-325-3334
 www.wre.com

PBS

PRELIMINARY

BRUNEAU ROAD CROSSING
 KENNEBEC, WA

SITE AND UTILITY PLAN

KENNEBEC TERMINAL RAILROAD

425 SE 2ND AVE., SUITE 208
 PORTLAND, OR 97214
 (503) 850-4011

DATE:	NOV 11 2008	SCALE:	AS SHOWN
BY:	WRE	CHECKED BY:	WRE
PROJECT:	C1	DATE:	NOV 11 2008
NO.	1	OF	1

AST CANTILEVER WITH 4 SETS OF LIGHTS AND 2 R15-1 CROSSBUCKS
 ADD CONCRETE BARRIER, REALIGN SIDEWALK REF C1

FIELD INLET AND PERF PIPE

6x6 EQUIP. HOUSE, 20.84' BACK FROM ROAD, 20.73' FROM TRACK, USE UNDER GROUND POWER BASE.

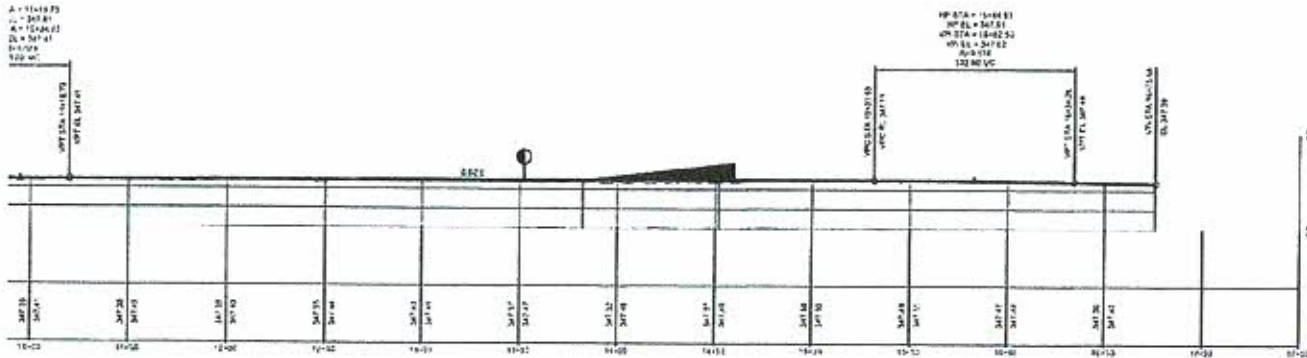
X-BUCK / STOP SIGN, LOCATED ON S. SIDE OF TRACK REF C1

12 (5'-1") OMEGA CONCRETE CROSSING PANELS, 97.5'

ADD CONCRETE BARRIER, REALIGN SIDEWALK REF C1

38'-1" DOUBLE-MAST CANTILEVER WITH 4 SETS OF LIGHTS, 2 SETS OF BACK LIGHTS AND 2 R15-1 CROSSBUCKS. AIM ONE SET OF LIGHTS AT WEST BOUND BRUNEAU AVE. AND THE OTHER AT EAST BOUND BRUNEAU AVE.

SAWCUT LINE AND LIMITS OF REPAVING

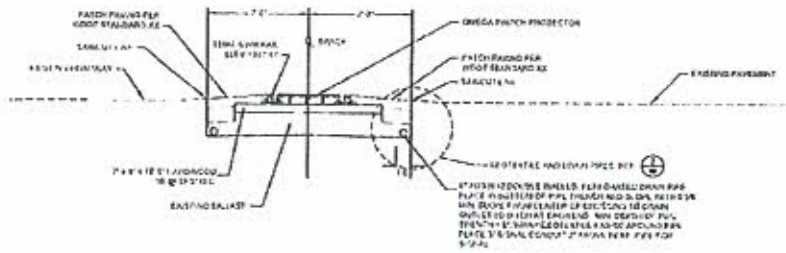
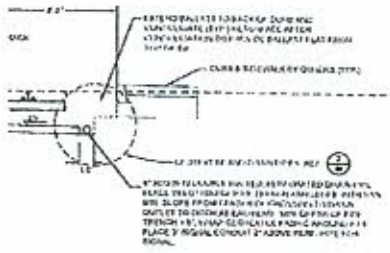


REDUCED DRAWINGS - DO NOT SCALE

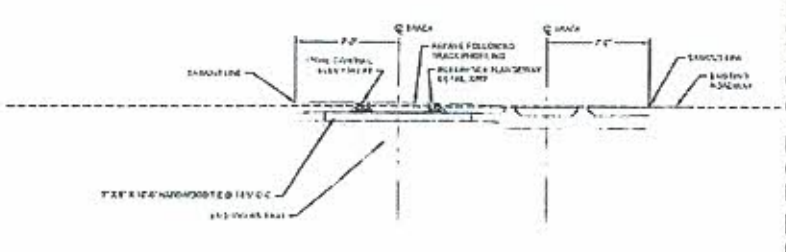
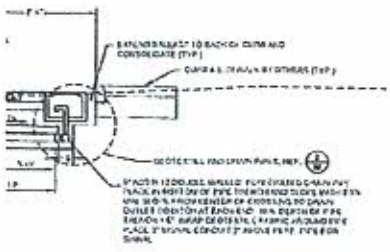


PRELIMINARY NOT FOR CONSTRUCTION
 BRUNEAU ROAD CROSSING
 KENNEBEC, WA
 PLAN & PROFILE
 KENNEBEC TERMINAL RAILROAD
 425 SE 3RD AVE., SUITE 308
 PORTLAND, OR 97214
 (503) 838-0001

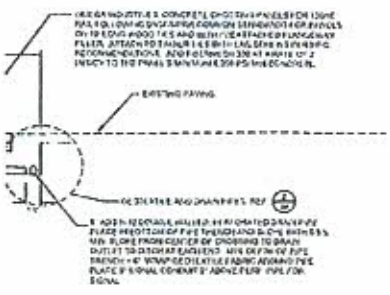
PROJECT NO.	1108
SHEET NO.	R1
DATE	11/17/06
SCALE	AS SHOWN
DRAWN BY	W.M.
CHECKED BY	J.P.
APPROVED BY	J.P.



4 TRACK SECTION
STA. 19+60



5 TRACK SECTION
STA. 14+00



Thomas W. Wilson, P.E.
Civil Engineer
No. 10000 S.W. 22nd Ave
Suite 100
Miami, FL 33187
305-554-1000
www.twwilson.com



PRELIMINARY NOT FOR CONSTRUCTION

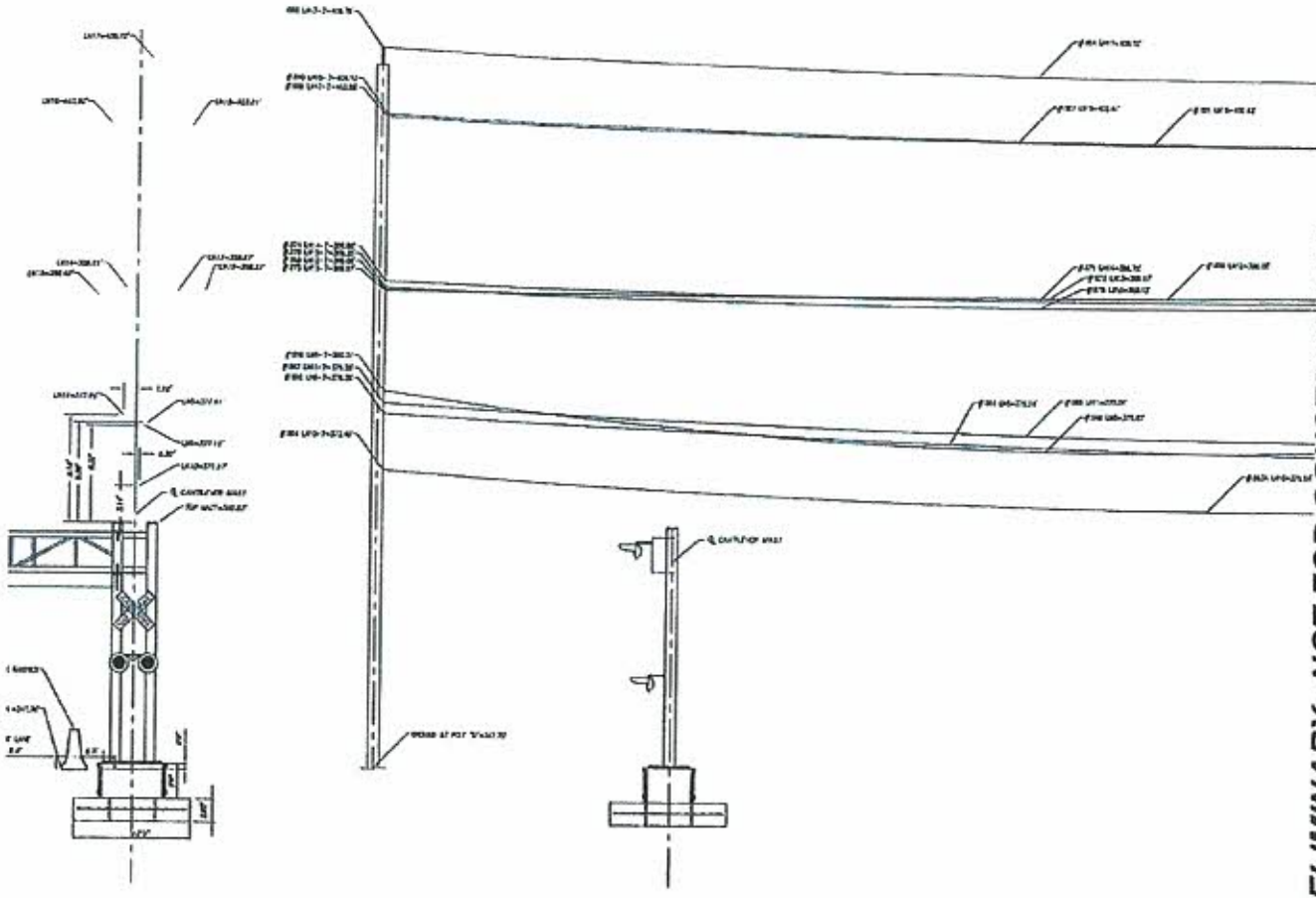
BRUNEAU ROAD CROSSING
KENNEWICK, WA

KENNEWICK TERMINAL RAILROAD
425 SE 3RD AVE., SUITE 206
PORTLAND, OR 97214
(503) 816-6211

NO.	DATE	BY	CHKD.	DESCRIPTION
1				
2				
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20				

R3

REDUCED DRAWINGS - DO NOT SCALE



10N - FRONT

2 NORTH CANTILEVER ELEVATION - RIGHT
 STA. 12+59.71, 83.6' L.T.

Thomas W. Wilson, P.E.
 2245 26th Street, Astoria,
 OR 97103
 (503) 325-1234
 TWP ENGINEERING
 CIVIL & STRUCTURAL ENGINEERING



PRELIMINARY NOT FOR CONSTRUCTION

KENNEWICK ROAD CROSSING
 KENNEWICK, WA
 NORTH CANTILEVER
 ELEVATIONS

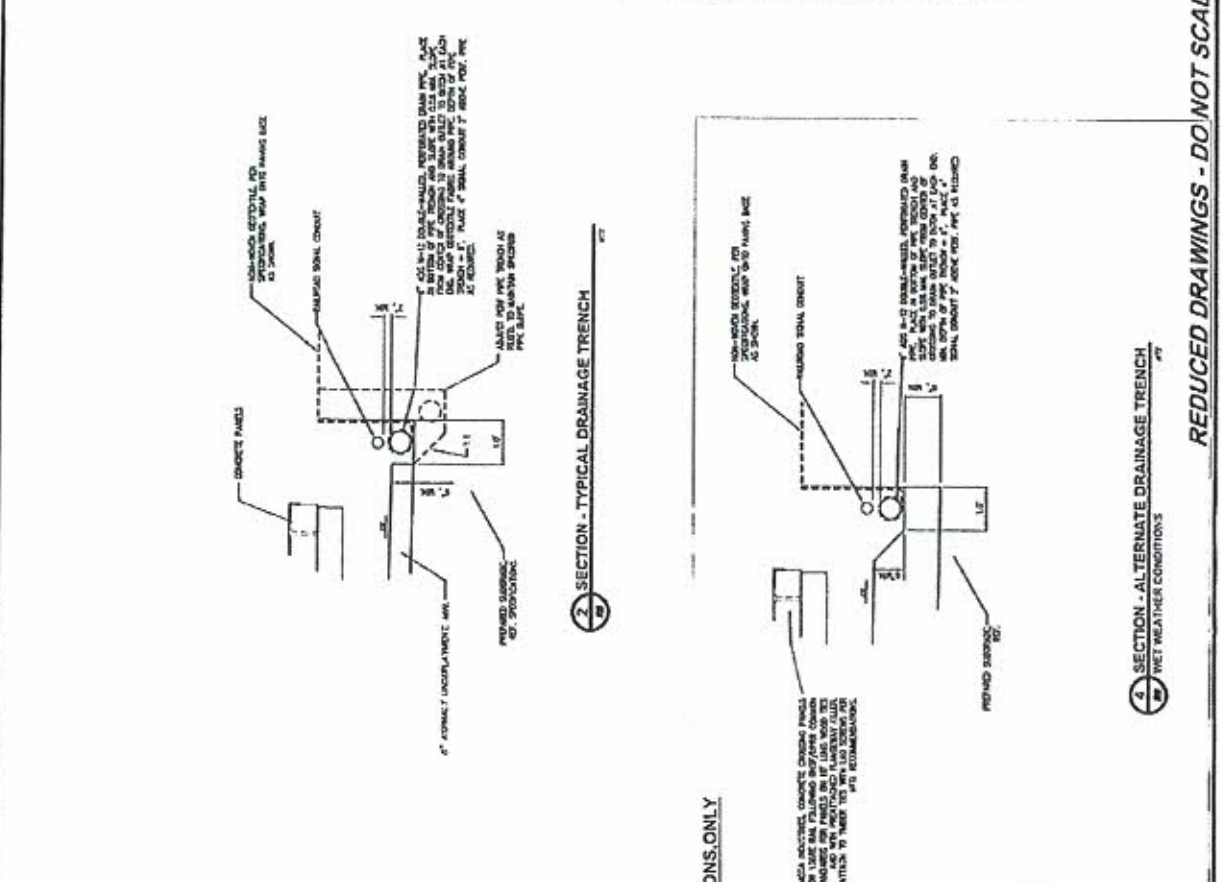
KENNEWICK TERMINAL RAILROAD
 425 SE 3RD AVE., SUITE 208
 PORTLAND, OR 97214
 (503) 818-5511

NO.	DATE	DESCRIPTION	BY	CHKD.

REDUCED DRAWINGS - DO NOT SCALE

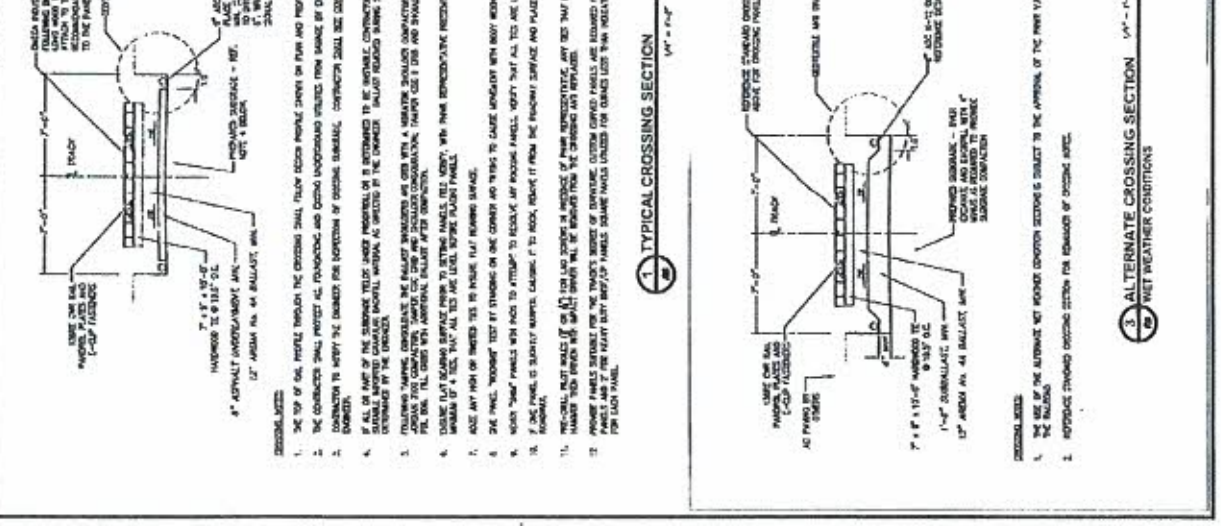
R4

PRELIMINARY NOT FOR CONSTRUCTION



1. TYPICAL CROSSING SECTION
 1/4" = 1'-0"

1. THE TOP OF THE TRENCH THROUGH THE CROSSING SHALL FOLLOW EXISTING FINISH GRADE OF PAVEMENT AND CURBS.
 2. THE CROSSING SHALL PROTECT ALL FOUNDATIONS AND OTHER UNDERGROUND UTILITIES FROM DAMAGE BY OVERFLOW WATER.
 3. CONTRACTOR SHALL VERIFY THE DIMENSIONS FOR DEPTH OF EXISTING SUBGRADE. CONTRACTOR SHALL NOT COVER UP THE EXISTING UNITS, AFTER NOTIFICATION BY OWNER.
 4. IF ALL OR PART OF THE EXISTING TRENCH IS DAMAGED OR IS OTHERWISE TO BE REPAIRED, CONTRACTOR SHALL DEMONSTRATE, PLACE, AND COMPLY WITH ALL CITY AND COUNTY REQUIREMENTS FOR REPAIR. CONTRACTOR SHALL FURNISH ALL MATERIALS AND LABOR FOR REPAIR.
 5. FOLLOWING "TYPICAL" CROSSING SECTION SHALL BE CONSIDERED AS ONE WITH A VARIATION SHOULD CONTRACTOR ACTING IN THE INTEREST OF THE PROJECT. ALL DIMENSIONS SHALL BE IN ACCORDANCE WITH THE CITY AND COUNTY REQUIREMENTS FOR DEPTH OF EXISTING SUBGRADE. CONTRACTOR SHALL FURNISH ALL MATERIALS AND LABOR FOR REPAIR.
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 7. THE PANELS TO BE SUBSTITUTED SHALL BE IDENTICAL TO THE PANELS SHOWN AND SHALL BE IDENTICAL TO THE PANELS SHOWN.
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2. SECTION - TYPICAL DRAINAGE TRENCH
 1/4" = 1'-0"

1. THE TOP OF THE TRENCH THROUGH THE CROSSING SHALL FOLLOW EXISTING FINISH GRADE OF PAVEMENT AND CURBS.
 2. THE CROSSING SHALL PROTECT ALL FOUNDATIONS AND OTHER UNDERGROUND UTILITIES FROM DAMAGE BY OVERFLOW WATER.
 3. CONTRACTOR SHALL VERIFY THE DIMENSIONS FOR DEPTH OF EXISTING SUBGRADE. CONTRACTOR SHALL NOT COVER UP THE EXISTING UNITS, AFTER NOTIFICATION BY OWNER.
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3. ALTERNATE CROSSING SECTION
 1/4" = 1'-0"
 WET WEATHER CONDITIONS

4. SECTION - ALTERNATE DRAINAGE TRENCH
 WET WEATHER CONDITIONS

REDUCED DRAWINGS - DO NOT SCALE



Thomas W. Winters, P.E.
 11000 SW 15th St.
 Portland, Oregon 97219
 503.747.4400



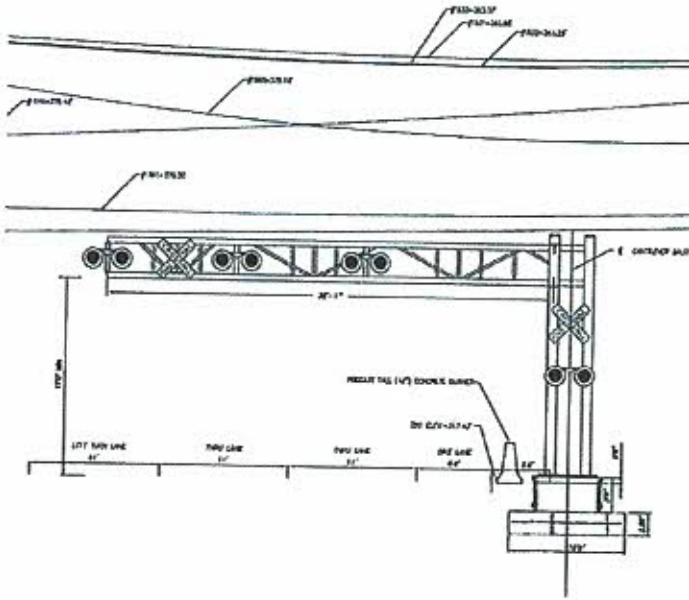
PRELIMINARY NOT FOR CONSTRUCTION

KENNEWICK TERMINAL RAILROAD
 BRUNEAU ROAD CROSSING
 KENNEWICK, WA
 SOUTH CANTILEVER
 ELEVATIONS

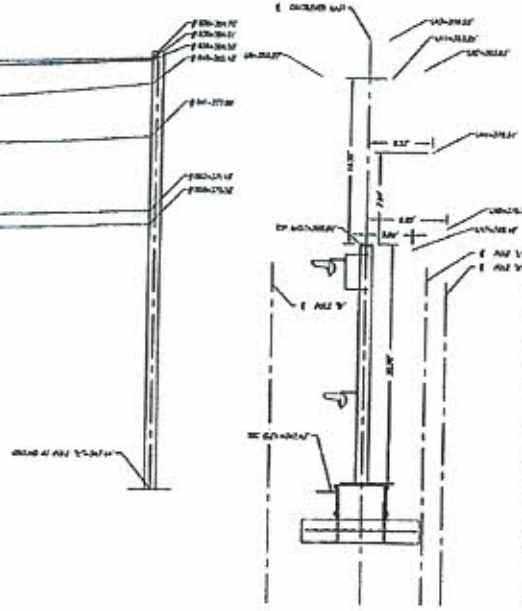
425 SE 3RD AVE., SUITE 306
 PORTLAND, OR 97214
 (503) 815-6071

DATE	BY	CHKD	APP'D

R5

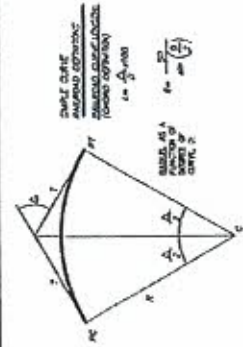


1 SOUTH CANTILEVER - FRONT
 STA. 13+49.37, 15.0' RT.



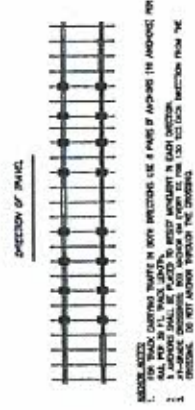
2 SOUTH CANTILEVER - RIGHT
 STA. 13+49.37, 15.0' RT.

REDUCED DRAWINGS - DO NOT SCALE



NOTE: THE PLAN CURVE GEOMETRY IS BASED UPON THE NORMAL. THE DRAWING ON THE PLAN IS BASED UPON THE ACTUAL LENGTH OF THE CHAIRS, TIES AND ANCHOR PATTERNS. THE CURVE GEOMETRY IS BASED UPON THE ACTUAL LENGTH OF THE CHAIRS, TIES AND ANCHOR PATTERNS. THE CURVE GEOMETRY IS BASED UPON THE ACTUAL LENGTH OF THE CHAIRS, TIES AND ANCHOR PATTERNS. THE CURVE GEOMETRY IS BASED UPON THE ACTUAL LENGTH OF THE CHAIRS, TIES AND ANCHOR PATTERNS.

1 CURVE GEOMETRY #1.15



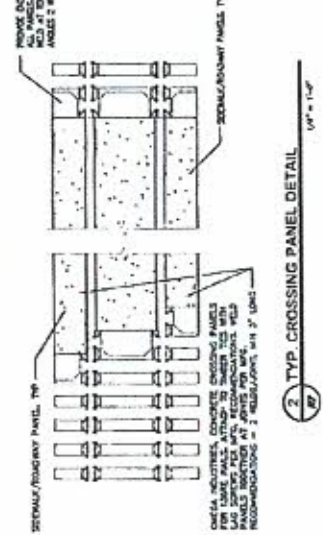
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2 TYP. CROSSING PANEL DETAIL #1.16



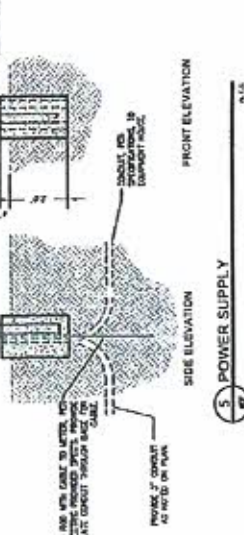
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3 ANCHOR PATTERN - TIMBER TIES #1.17



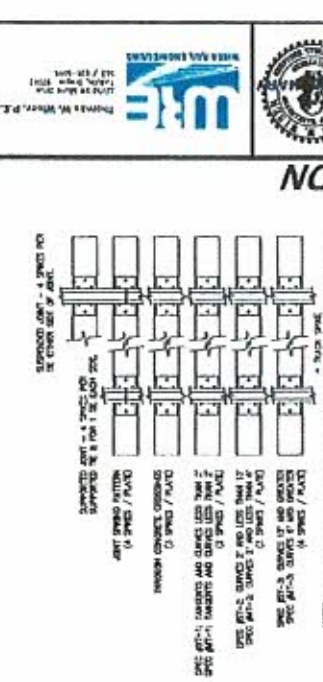
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4 POWER SUPPLY #1.18



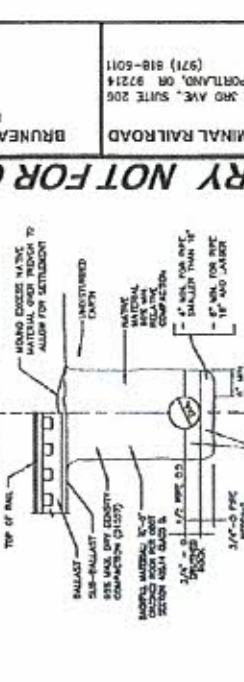
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5 PIPE BEDDING DETAIL #1.19



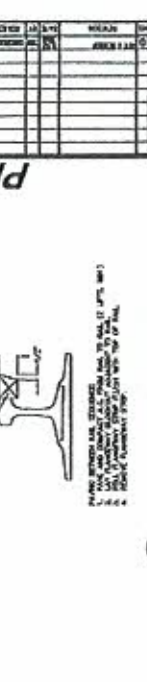
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6 ASPHALT PAVING FLANGEWAY #1.20



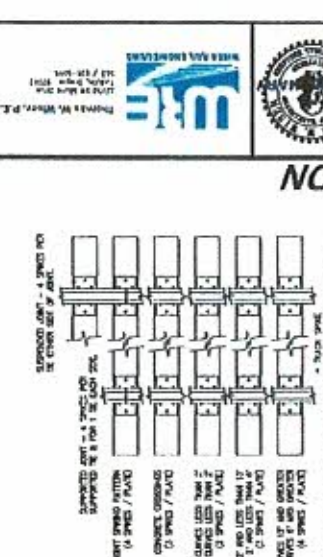
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7 TRENCH DRAIN DETAIL #1.21



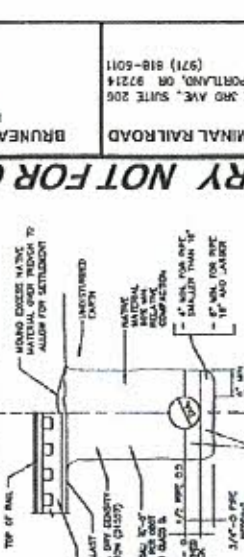
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8 OMEGA SWITCH POINT DETAIL #1.22



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9 TYP. SPIKING PATTERNS #1.23



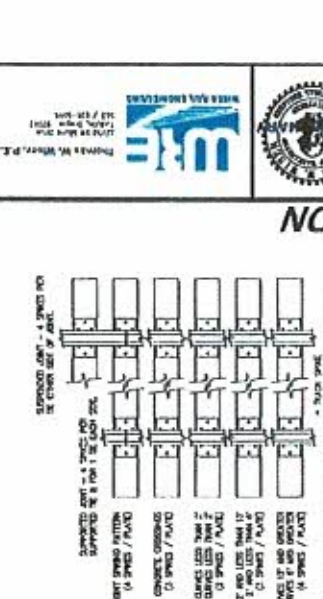
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10 PIPE BEDDING DETAIL #1.24



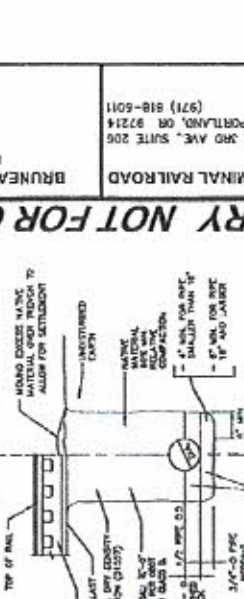
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11 TYPICAL SECTION #1.25



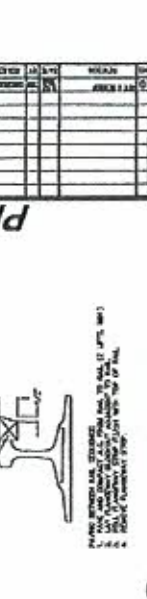
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12 OMEGA SWITCH POINT DETAIL #1.26



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13 TYP. SPIKING PATTERNS #1.27



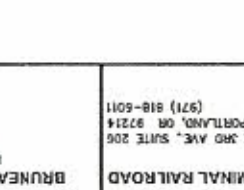
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14 PIPE BEDDING DETAIL #1.28



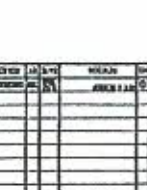
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15 ASPHALT PAVING FLANGEWAY #1.29



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16 TRENCH DRAIN DETAIL #1.30



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17 OMEGA SWITCH POINT DETAIL #1.31

THOMAS W. WHELAN, P.E.
WHEELAN ENGINEERING
1101 S. 19TH AVENUE
SUITE 100
DENVER, CO 80202
(303) 733-8800



BRUNEAU ROAD CROSSING
KENNEWICK, WA
425 SE 3RD AVE., SUITE 206
PORTLAND, OR 97214
(503) 818-6011

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From: [Paul Didelius](#)
To: [Young, Betty \(UTC\)](#)
Cc: [Raezer, Connie](#); [Nizam, Ahmer](#)
Subject: Re: Petition to Modify Devices at 808925X
Date: Friday, February 14, 2020 2:39:59 PM
Attachments: [CCF_000064\(1\).pdf](#)

Hello - note attached signed doc. I updated a few minor items - if you need a clean copy please just adjust on your end and I can re-sign.

Thank you,

Paul Didelius

Owner / President
Columbia Rail Group
Carriers **CCET CWW WRL YCR**

pd@columbiarail.com
(509) 492-3553

709 N. 10th Ave.
Walla Walla, WA 99362

On Mon, Feb 3, 2020 at 12:39 PM Young, Betty (UTC) <betty.young@utc.wa.gov> wrote:

That works – thanks Connie!

Betty

From: Raezer, Connie [mailto:RaezerC@wsdot.wa.gov]
Sent: Monday, February 3, 2020 12:17 PM
To: Young, Betty (UTC) <betty.young@utc.wa.gov>
Cc: Paul Didelius <pd@columbiarail.com>; Paul Didelius <pd@frontierrail.com>; Nizam, Ahmer <nizama@wsdot.wa.gov>
Subject: RE: Petition to Modify Devices at 808925X

I went back to my email to recall why we had entered the data this way on the Petition.

I believe the “dual name” situation came up in relationship to the agreement. This was to address all of the affiliated companies employees being able to work under the agreement. This can be addressed in the Construction Agreement and therefore I can provide the

attached replacement pages for the Petition just listing KET.

Please let me know if this is sufficient.

Connie Raezer

Desk: 360-705-7459

Cell: 360-701-2242

Please visit the: [Highway-Railroad Coordination Informational Webpage](#)

From: Young, Betty (UTC) <betty.young@utc.wa.gov>
Sent: Monday, February 3, 2020 11:34 AM
To: Raezer, Connie <RaezerC@wsdot.wa.gov>
Cc: Paul Didelius <pd@columbiarail.com>; Paul Didelius <pd@frontierrail.com>; Nizam, Ahmer <nizama@wsdot.wa.gov>
Subject: RE: Petition to Modify Devices at 808925X
Importance: High

All – before we proceed with this petition, I need to be clear about which railroad(s) is the correct respondent.

According to information we received from Rob Didelius, KET LLC is not a d/b/a of CWW, LLC, but is a whole different company (see attached). Should the respondent be KET LLC, dba Kennewick Terminal Railway?

Thanks for your help.

Betty

From: Raezer, Connie [<mailto:RaezerC@wsdot.wa.gov>]
Sent: Friday, January 31, 2020 9:01 AM
To: Young, Betty (UTC) <betty.young@utc.wa.gov>

Cc: Paul Didelius <pd@columbiarail.com>; Paul Didelius <pd@frontierrail.com>; Nizam, Ahmer <nizama@wsdot.wa.gov>

Subject: Petition to Modify Devices at 808925X

Ms. Young,

WSDOT is submitting a Petition to modify the warning devices at crossing 808925X on SR 397 in Kennewick. A Diagnostic Team conducted a site visit on September 17, 2014. The UTC was represented by Paul Curl. The crossing currently has passive protection only, is a five lane crossing, and has a very high average daily traffic count. There is an accident history that was a consideration. After concurrence of the Diagnostic Team on the safety improvements, the railroad and WSDOT entered into a design agreement funded under federal funds Section 130. Design was completed and is submitted with this Petition. The railroad and WSDOT have negotiated a construction agreement. The construction is fully funded under Section 130. While we finalize the signature of that agreement I would like UTC to begin the Petition process.

If there are any questions, please contact me.

Connie Raezer

WSDOT Railroad Liaison

310 Maple Park Avenue SE, 2B

Mailstop 47329

Olympia, WA 98504-7329

360-705-7459 Desk

360-701-2242 Cell

Please visit the: [Highway-Railroad Coordination Informational Webpage](#)

From: [Raezer, Connie](#)
To: [Paul Didelius](#); [Young, Betty \(UTC\)](#)
Cc: [Nizam, Ahmer](#)
Subject: RE: Petition to Modify Devices at 808925X
Date: Saturday, February 15, 2020 1:26:57 PM

Betty, WSDOT has no concerns with edits to the name/description of the railroad company provided by Paul.

Connie Raezer

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Cell: 360-701-2242

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Cc: Raezer, Connie <RaezerC@wsdot.wa.gov>; Nizam, Ahmer <nizama@wsdot.wa.gov>
Subject: Re: Petition to Modify Devices at 808925X

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Cc: Paul Didelius <pd@columbiarail.com>; Paul Didelius <pd@frontierrail.com>; Nizam, Ahmer <nizama@wsdot.wa.gov>
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