



CITY OF KENT

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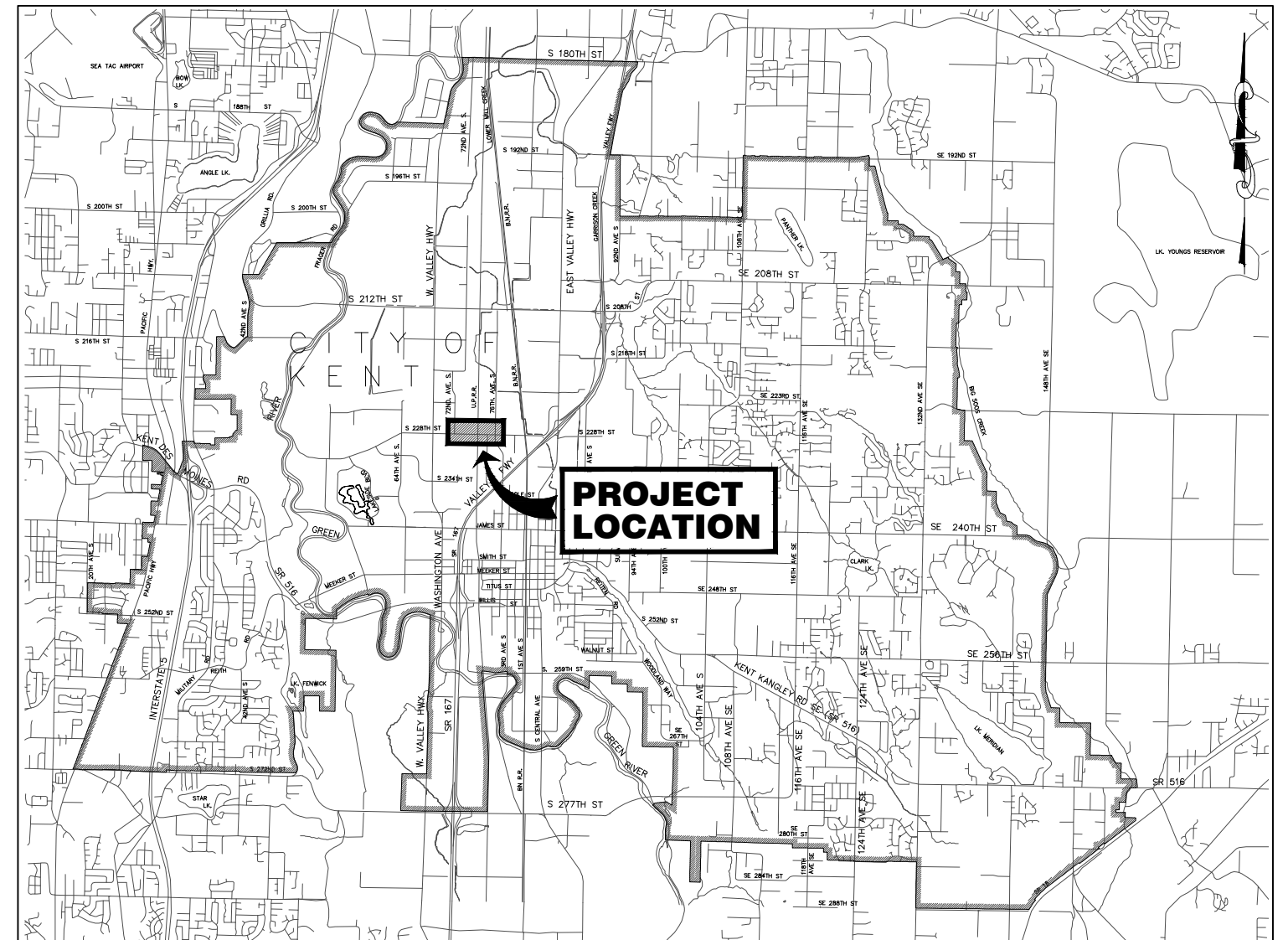
PUBLIC WORKS PROJECT

S. 228th ST. GRADE SEPARATION AT UNION PACIFIC RAILROAD

JOB NUMBER 07-3022

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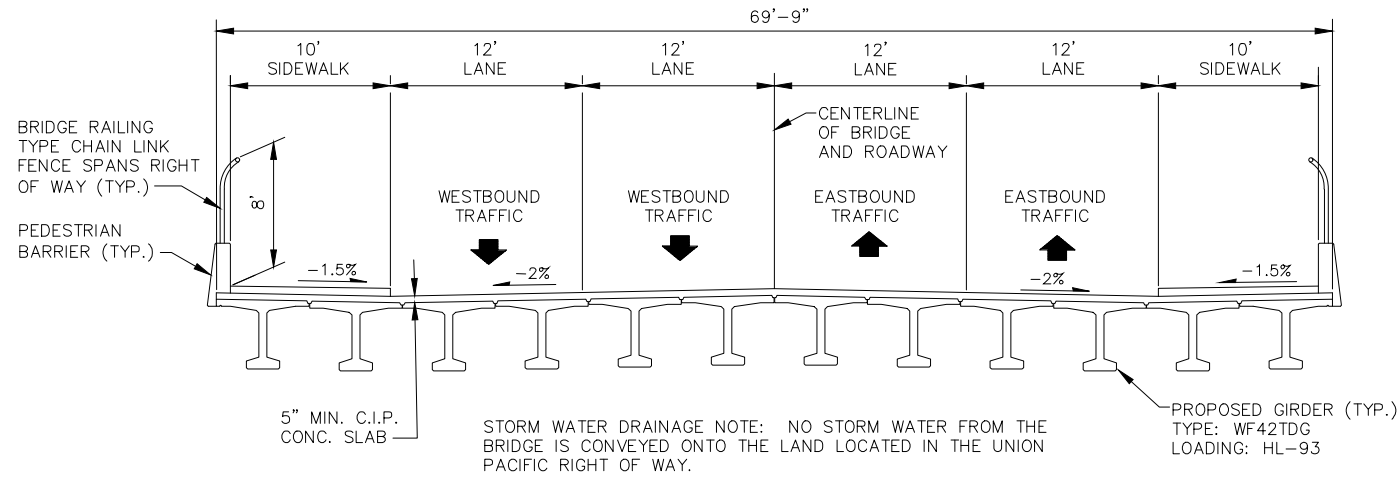


VICINITY MAP
NOT TO SCALE

100% PLANS FOR RAILROAD AGREEMENT

GENERAL NOTES

- PRIOR TO ANY CONSTRUCTION ACTIVITY, THE CONTRACTOR SHALL ATTEND A SCHEDULED PRE-CONSTRUCTION CONFERENCE WITH THE CITY OF KENT CONSTRUCTION INSPECTION PERSONNEL. PHONE (253) 856-5500 TO SCHEDULE CONFERENCE.
- ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH THE 2009 EDITION OF THE CITY OF KENT CONSTRUCTION STANDARDS, THE WSDOT STANDARD SPECIFICATIONS, 2014 EDITION AND THE KENT SPECIAL PROVISIONS FOR THIS PROJECT.
- THE CONTRACTOR SHALL COMPLY WITH ALL PUBLIC CONVENIENCE AND SAFETY AS DESCRIBED IN SECTION 1-07.23 TO SECTION 1-07.23(2) OF THE KENT SPECIAL PROVISIONS FOR STORING OF EQUIPMENT AND MATERIALS DURING NON-WORKING HOURS.
- THE CONTRACTOR SHALL KEEP STREETS CLEAN AT ALL TIMES BY STREET SWEEPING WHEN NECESSARY, OR WHEN DIRECTED BY THE CONSTRUCTION ENGINEER.
- ALL ITEMS OF WORK NOT LISTED IN THE BID PROPOSAL WHICH ARE SHOWN ON THE CONTRACT DRAWINGS AND ARE REQUIRED TO COMPLETE THE WORK THAT IS SHOWN SHALL BE CONSIDERED INCIDENTAL TO THE LISTED BID ITEMS.
- ALL TREES AND VEGETATION WITHIN THE PROJECT LIMITS SHALL BE REMOVED AS NOTED. ALL OTHER TREES SHALL REMAIN IN PLACE. CONTRACTOR SHALL PROTECT REMAINING TREES FROM DAMAGE RESULTING FROM CONSTRUCTION ACTIVITIES INCLUDING DAMAGE TO ROOT SYSTEM.
- THE CONTRACTOR SHALL RESTORE ALL LANDSCAPED AREAS ON-SITE AND ADJACENT TO THE SITE THAT WERE DISTURBED BY CONSTRUCTION TO ORIGINAL CONDITION UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE COSTS FOR GENERAL RESTORATION AND CLEAN UP REQUIRED IN COMPLIANCE WITH THE SPECIFICATIONS FOR THIS PROJECT SHALL BE CONSIDERED INCIDENTAL TO AND INCLUDED IN THE UNIT CONTRACT PRICE OF OTHER BID ITEMS EXCEPT FOR THOSE ITEMS INCLUDED IN THE PROPOSAL.
- UNLESS OTHERWISE NOTED, THE WORD "REMOVE" OR "REMOVAL" IN THE CONSTRUCTION NOTES AND SHOWN ON THE PLANS MEANS REMOVAL, HAUL AND PROPER OFF-SITE DISPOSAL BY THE CONTRACTOR AT THE CONTRACTORS EXPENSE.
- THESE ISSUED-FOR-CONSTRUCTION PLANS, THE APPROVED TRAFFIC CONTROL PLANS, THE APPROVED TEMPORARY EROSION/SEDIMENTATION CONTROL PLANS, THE STANDARD SPECIFICATIONS AND KENT SPECIAL PROVISIONS SHALL BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
- UNLESS STATED OTHERWISE THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE MEANS, METHODS AND SEQUENCE OF CONSTRUCTION AND FOR THE SAFETY OF THE WORKERS.
- ALL EXISTING UTILITIES SHALL REMAIN IN SERVICE UNLESS OTHERWISE NOTED.
- IDENTIFICATION, LOCATION, MARKING AND RESPONSIBILITY FOR UNDERGROUND FACILITIES OR UTILITIES IS GOVERNED BY THE PROVISIONS OF CHAPTER 19.122, REVISED CODE OF WASHINGTON.
- THE CONTRACTOR SHALL CONTACT THE UNDERGROUND UTILITIES LOCATION SERVICE (811) AT LEAST TWO WORKING DAYS PRIOR TO CONSTRUCTION. THE OWNER OR HIS REPRESENTATIVE AND THE ENGINEER SHALL BE CONTACTED IMMEDIATELY IF A CONFLICT EXISTS. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING AND/OR RECORDING THE LOCATION MARKS.
- CAUTION - EXTREME HAZARD - OVERHEAD AND UNDERGROUND ELECTRICAL SERVICE LINES ARE GENERALLY NOT SHOWN ON THE DRAWINGS. THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE EXTENT OF ANY HAZARD CREATED BY OVERHEAD OR UNDERGROUND ELECTRICAL POWER IN ALL AREAS AND SHALL FOLLOW PROCEDURES DURING CONSTRUCTION AS REQUIRED BY LAW AND REGULATION. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL MEET WITH UTILITY OWNERS AND DETERMINE THE EXTENT OF HAZARD AND REMEDIAL MEASURES AND SHALL TAKE WHATEVER PRECAUTIONS THAT MAY BE REQUIRED.
- ALL LOCATIONS OF EXISTING UTILITIES SHOWN HEREON HAVE BEEN ESTABLISHED BY FIELD SURVEY OR OBTAINED FROM AVAILABLE RECORDS AND SHOULD THEREFORE BE CONSIDERED APPROXIMATE ONLY AND NOT NECESSARILY COMPLETE. IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO INDEPENDENTLY VERIFY THE ACCURACY OF ALL UTILITY LOCATIONS SHOWN AND TO FURTHER DISCOVER AND AVOID ANY OTHER UTILITIES NOT SHOWN HEREON WHICH MAY BE AFFECTED BY THE IMPLEMENTATION OF THIS PLAN.
- THE CONTRACTOR IS RESPONSIBLE FOR ALL SHORING AND BRACING REQUIRED TO PROTECT AND SUPPORT EXISTING UNDERGROUND UTILITIES AND STRUCTURES DURING CONSTRUCTION.
- ANY OPEN CUTS OF EXISTING PUBLIC ROADWAYS SHALL BE BACKFILLED AND COMPACTED IN ACCORDANCE WITH CITY OF KENT STANDARDS. ALL CUTS INTO EXISTING ASPHALT SHALL BE ALONG NEAT, CONTINUOUS, SAW CUT LINES. A 3" DEPTH TEMPORARY HOT MIX PATCH MUST BE PLACED IMMEDIATELY AFTER BACKFILL AND COMPACTION. UNLESS OTHERWISE SHOWN ON THESE PLANS OR DIRECTED BY THE ENGINEER, THE EXISTING SURFACING MUST BE REPLACED IN KIND (OR 3 INCHES OF COMPACTED HMA PG64-22, CLASS 1/2", ASPHALT CONCRETE, WHICHEVER IS GREATER) WITHIN 30 DAYS OF TEMPORARY HOT MIX PATCHING.
- THE CONTRACTOR SHALL CLOSELY FOLLOW REQUIREMENTS SET FOR WORKING HOURS, DETOUR, WARNING SIGNS AND NOTIFICATION OF ROAD ALTERATIONS TO THE POLICE AND/OR OTHER EMERGENCY SERVICES.
- STATIONS OR DISTANCES NOTED FOR STORM SEWER CATCH BASINS AND SANITARY SEWER MANHOLES ARE SHOWN TO THE CENTER OF STRUCTURE UNLESS OTHERWISE NOTED. PIPE LENGTHS AND SLOPES ARE CALCULATED FROM THE CENTER OF THE STRUCTURES.
- ALL UTILITY LIDS, COVERS, ETC. SHALL BE ADJUSTED TO GRADE.
- IT IS ILLEGAL UNDER WASHINGTON STATE ADMINISTRATIVE CODE 332-120 TO WILLFULLY DESTROY SURVEY MARKERS, STAKES, MARKS, AND OTHER REFERENCE POINTS SET BY CITY FORCES, AND EXISTING CITY, STATE OR FEDERAL MONUMENTATION, SHALL BE CAREFULLY PRESERVED BY THE CONTRACTOR. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IF IT BECOMES APPARENT THAT A SURVEY MARKER WILL BE DISTURBED DUE TO CONSTRUCTION. THE CONTRACTOR WILL ALLOW AMPLE TIME FOR CITY SURVEY DEPARTMENT PERSONNEL TO ACQUIRE ADEQUATE INFORMATION SO THAT THE MONUMENT MAY BE REPLACED IN ITS ORIGINAL POSITION AFTER CONSTRUCTION.
- DRIVEWAYS SHALL REMAIN OPEN AT ALL TIMES DURING BUSINESS HOURS. THE CONTRACTOR SHALL COORDINATE HIS WORK SCHEDULE WITH THE PROPERTY OCCUPANTS TO DETERMINE THE BEST TIME TO FULLY OR PARTIALLY CLOSE THE DRIVEWAYS IN ORDER TO PERFORM HIS WORK.



A
4 **TYPICAL BRIDGE SECTION - LOOKING EAST**
NOT TO SCALE

UPRR INFORMATION:

CROSSING: PUBLIC
STATE: WASHINGTON
COUNTY: KING
CITY: KENT
MP: 168.01
SUBDIVISION: OREGON
STREET: S. 228th ST.
LAT: 47.397
LONG: -122.241

RAILROAD NOTES

- THE PROPOSED GRADE SEPARATION PROJECT SHALL NOT INCREASE THE QUANTITY AND/OR CHARACTERISTICS OF THE FLOW IN THE RAILROAD DITCHES AND/OR DRAINAGE STRUCTURES.
- THE ELEVATION OF THE EXISTING TOP-OF-RAIL PROFILE SHALL BE VERIFIED BEFORE BEGINNING CONSTRUCTION. ALL DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE RAILROAD PRIOR TO CONSTRUCTION.
- THE CONTRACTOR MUST SUBMIT A PROPOSED METHOD OF EROSION AND SEDIMENT CONTROL AND HAVE THE METHOD APPROVED BY THE RAILROAD.
- ALL SHORING SYSTEMS THAT IMPACT THE RAILROAD'S OPERATIONS AND/OR SUPPORTS THE RAILROAD'S EMBANKMENT SHALL BE DESIGNED AND CONSTRUCTION PER CURRENT RAILROAD GUIDELINES FOR TEMPORARY SHORING.
- ALL DEMOLITIONS WITHIN THE RAILROAD'S RIGHT-OF-WAY AND/OR DEMOLITION THAT MAT IMPACT THE RAILROAD'S TRACKS OR OPERATIONS SHALL BE IN COMPLIANCE WITH THE RAILROAD'S DEMOLITION GUIDELINES.
- ERECTION OVER THE RAILROAD'S RIGHT-OF-WAY SHALL BE DESIGNED TO CAUSE NO INTERRUPTION TO THE RAILROAD'S OPERATION, ENABLING THE TRACK(S) TO REMAIN OPEN TO TRAFFIC PER THE RAILROAD'S REQUIREMENTS.
- ALL PERSONNEL MUST CLEAR THE AREA WITHIN 25 FEET OF THE TRACK CENTERLINE AND SECURE ALL EQUIPMENT WHEN A TRAIN PASSES A WORK SITE.
- FALSE-WORK CLEARANCES SHALL COMPLY WITH MINIMUM CONSTRUCTION CLEARANCES.
- ALL PERMANENT CLEARANCES SHALL BE VERIFIED BEFORE PROJECT CLOSING.
- FOR RAILROAD COORDINATION PLEASE REFER TO THE RAILROAD MINIMUM REQUIREMENTS AS PART OF SPECIAL PROVISIONS.
- TEMPORARY HORIZONTAL AND VERTICAL CLEARANCES SHALL BE SHOWN ON THE PLANS AND EVERY EFFORT MUST BE MADE TO DESIGN FOR GREATER CLEARANCES. FALSE-WORK CLEARANCES SHALL COMPLY WITH MINIMUM CONSTRUCTION CLEARANCES. PROVIDE AND SHOW THE FOLLOWING MINIMUM CONSTRUCTION CLEARANCE DIAGRAM.

ABBREVIATIONS

ABAND	ABANDONED	GALV	GALVANIZED	(TYP.)	TYPICAL
AC	ASBESTOS CEMENT	FS	FINISHED SURFACE		
AP	ANGLE POINT	GB	GRADE BREAK		
CB	CATCH BASIN	HP	HIGH POINT		
CDF	CONTROLLED DENSITY FILL	IP	IRON PIPE		
C/L	CENTER LINE	JB	JUNCTION BOX		
CLR	CLEAR	MH	MANHOLE		
CMP	CORRUGATED METAL PIPE	MJ	MECHANICAL JOINT		
CI	CAST IRON	NA	NOT APPLICABLE		
CU	COPPER	NPT	NATIONAL PIPE THREAD		
DI	DUCTILE IRON	NTS	NOT TO SCALE		
EL	ELEVATION	PE	PLAIN END		
FL	FLOW LINE (GRADING)	RPM	RAISED PVMT. MARKING		
FL	FLANGE (WATER FITTINGS)	R/W	RIGHT OF WAY		
FM	FORCEMAIN	S/W	SIDEWALK		

DATUM



LEGEND

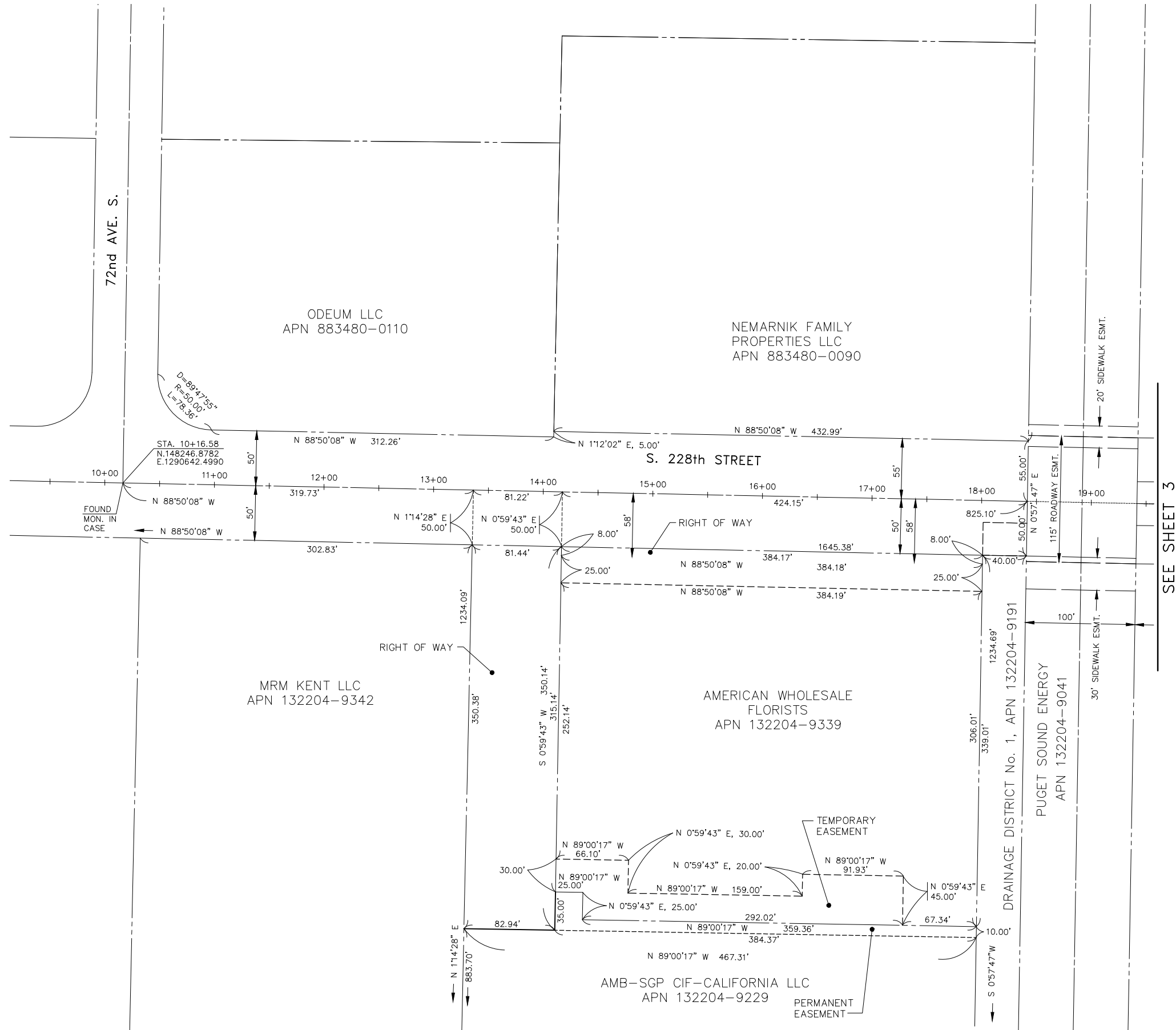
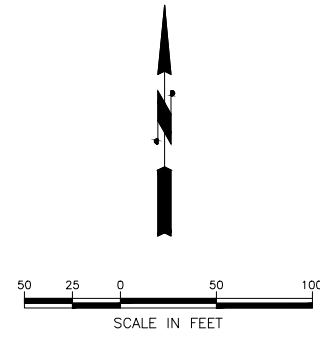
EXISTING FEATURES APPEAR SCREENED

	CONSTRUCTION NOTE
	SECTION LETTER OR DETAIL NUMBER
	SHEET NUMBER WHERE SECTION OR DETAIL IS REFERENCED
	SIGN
	MAILBOX
	WELL
	FENCE
	STREET LIGHT/LUMINAIRE
	UTILITY POLE W/GUY
	TELEPHONE MANHOLE/VAULT
	TELEPHONE PEDESTAL
	UNDERGROUND TELEPHONE LINE
	UNDERGROUND CATV LINE
	OVERHEAD POWER LINE
	UNDERGROUND POWER LINE
	UNDERGROUND FIBER OPTIC LINE
	EXISTING JUNCTION BOX
	NEW JUNCTION BOX
	POWER VAULT/CABINET
	GAS LINE AND VALVE
	FIRE HYDRANT
	IRRIGATION CONTROL BOX
	WATER METER
	VALVE POSITION INDICATOR
	FIRE DEPT. CONNECTION
	WATER VAULT
	WATERLINE AND VALVE
	STORM SEWER LINE & CATCH BASIN
	STORM SEWER LINE & MANHOLE
	FORCEMAIN
	SANITARY SEWER LINE & MANHOLE
	CENTERLINE
	DITCH FLOWLINE
	WETLAND BOUNDARY
	R/W LINE
	PROPERTY LINE AND OWNERSHIP HOOK
	MONUMENT IN CASE
	EXISTING EDGE OF PAVEMENT
	TRAFFIC SIGNAL CABLING
	CULVERT
	BUILDING
	EXISTING CONCRETE
	RAMP (DOWN IN DIRECTION OF ARROW)
	TRAFFIC SIGNAL
	PEDESTRIAN PUSH BUTTON
	DECIDUOUS TREE
	EVERGREEN TREE
	LIMITS OF PAVEMENT PLANE
	PEDESTRIAN BARRIER



LEGEND

- EXISTING CENTERLINE
- EXISTING RIGHT OF WAY LINE
- EXISTING PROPERTY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED PERMANENT EASEMENT
- PROPOSED TEMPORARY EASEMENT

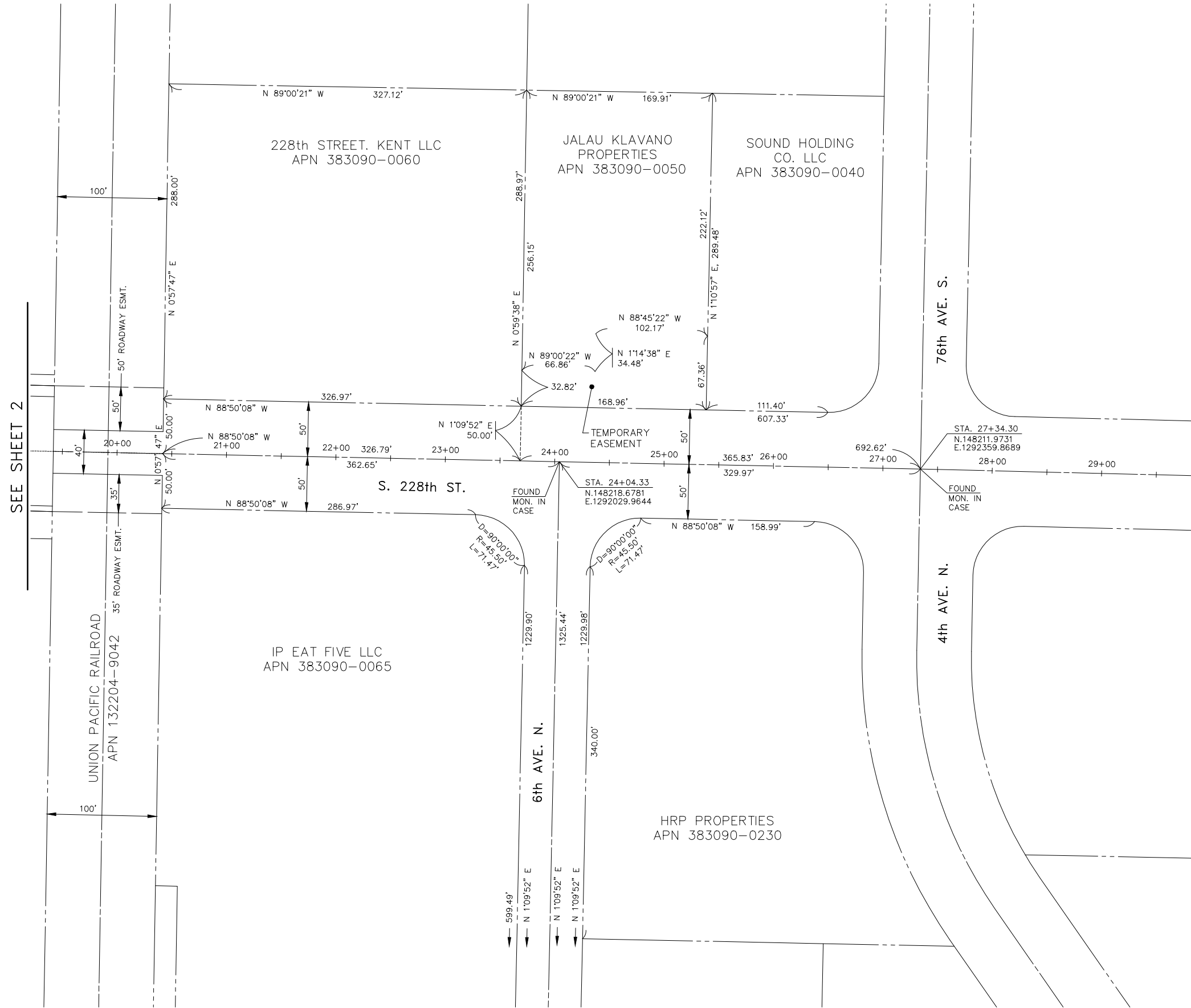
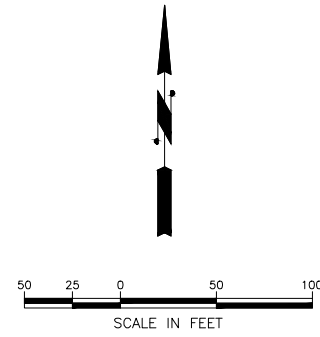


SEE SHEET 3

				DESIGNED: RW	CHECKED:	KENT PROJECT NO. 07-3022	SCALE: HORIZ: SHOWN VERT: NONE	BAR IS ONE INCH ON ORIGINAL DRAWING ADJUST SCALES ACCORDINGLY	 City of Kent Public Works Department Engineering Division	RIGHT OF WAY PLAN S. 228th STREET GRADE SEPARATION AT THE UNION PACIFIC RAILROAD	SHEET 2 OF 6
NO.	REVISION	BY	DATE	APPROVED:	CITY ENGINEER	DATE	4-19-16	0" = 1"			FILE NO.

LEGEND

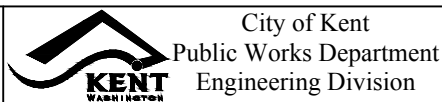
- EXISTING CENTERLINE
- EXISTING RIGHT OF WAY LINE
- EXISTING PROPERTY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED PERMANENT EASEMENT
- PROPOSED TEMPORARY EASEMENT



DESIGNED: RW	CHECKED:	KENT PROJECT NO. 07-3022	SCALE: HORIZ: SHOWN
DRAWN: RW	PROJECT ENGR:	CONST. MGMT. REVIEW	VERT: NONE
APPROVED:	CITY ENGINEER	DATE 4-19-16	BAR IS ONE INCH ON ORIGINAL DRAWING ADJUST SCALES ACCORDINGLY

NO.	REVISION	BY	DATE

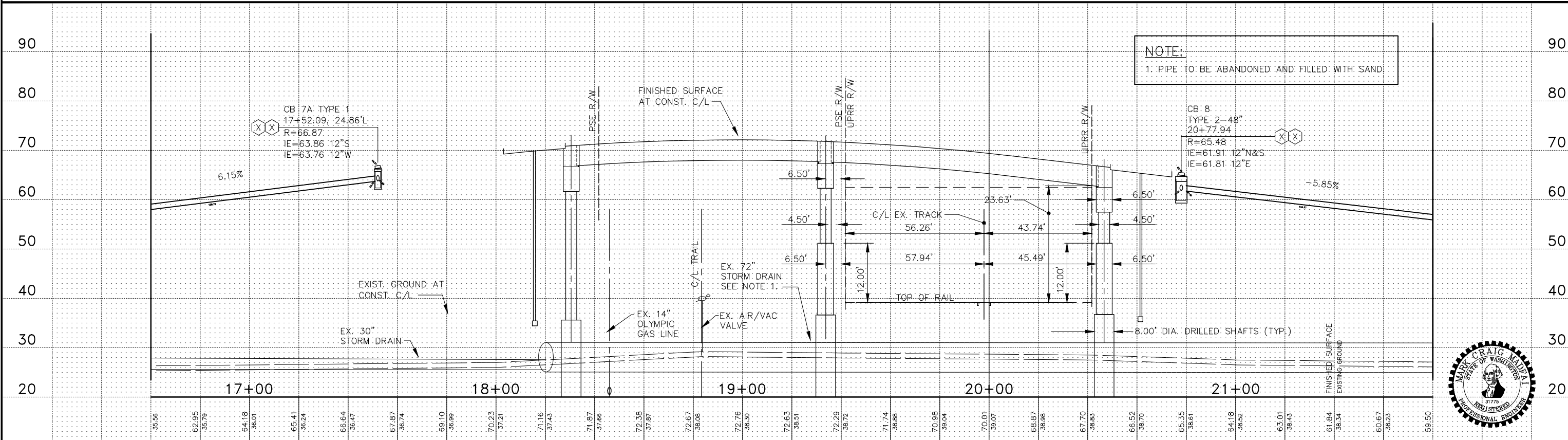
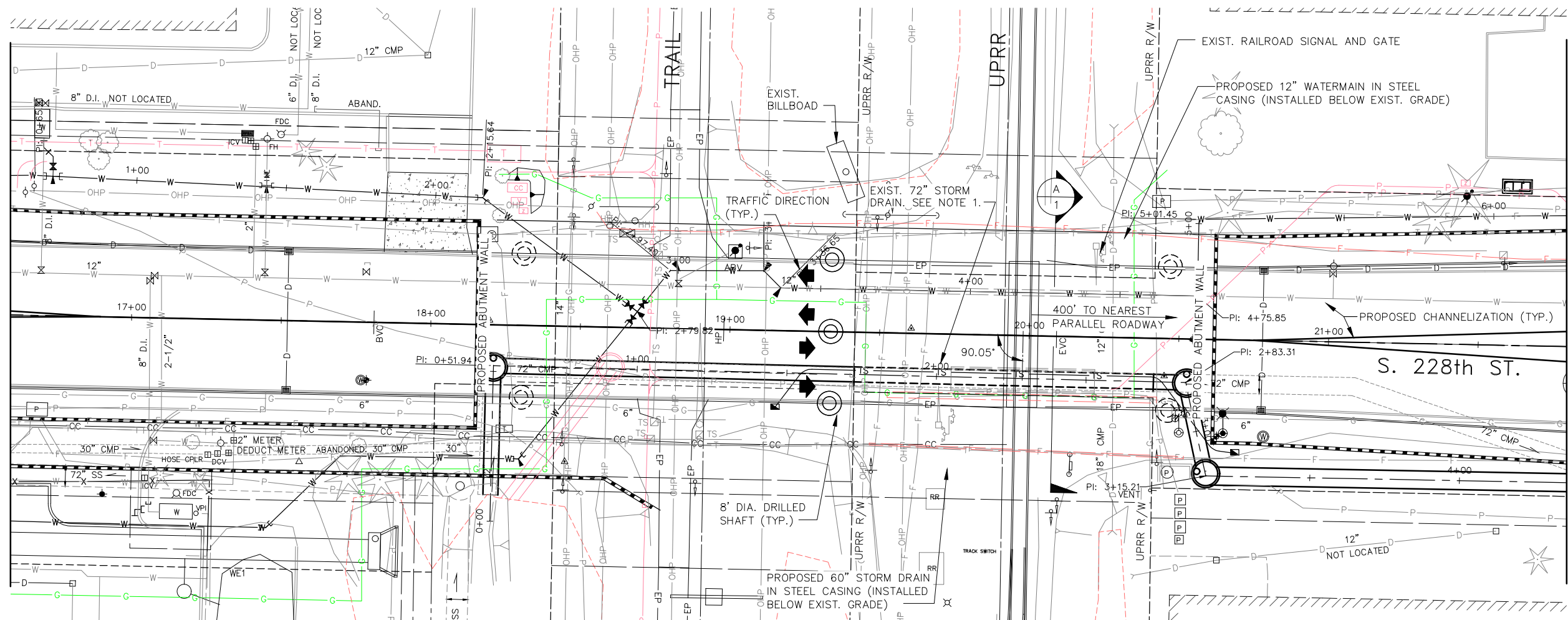
City of Kent
Public Works Department
Engineering Division



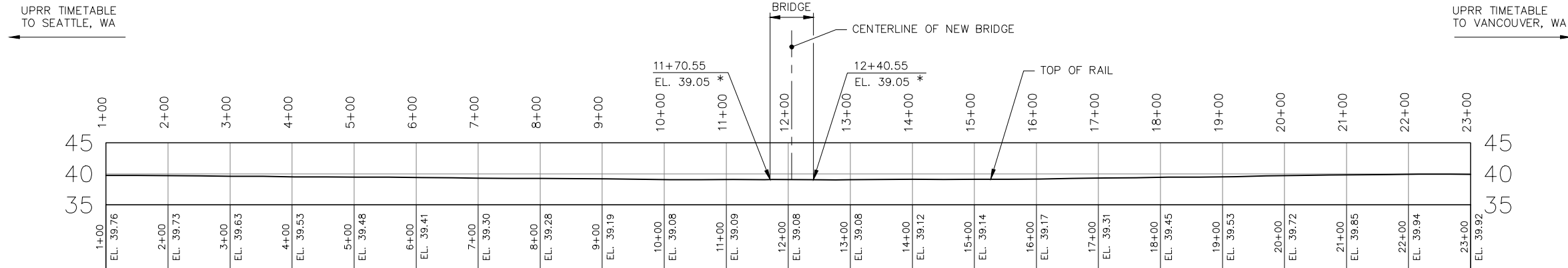
RIGHT OF WAY PLAN

**S. 228th STREET
GRADE SEPARATION
AT THE UNION PACIFIC RAILROAD**

SHEET	3 OF 6
FILE NO.	



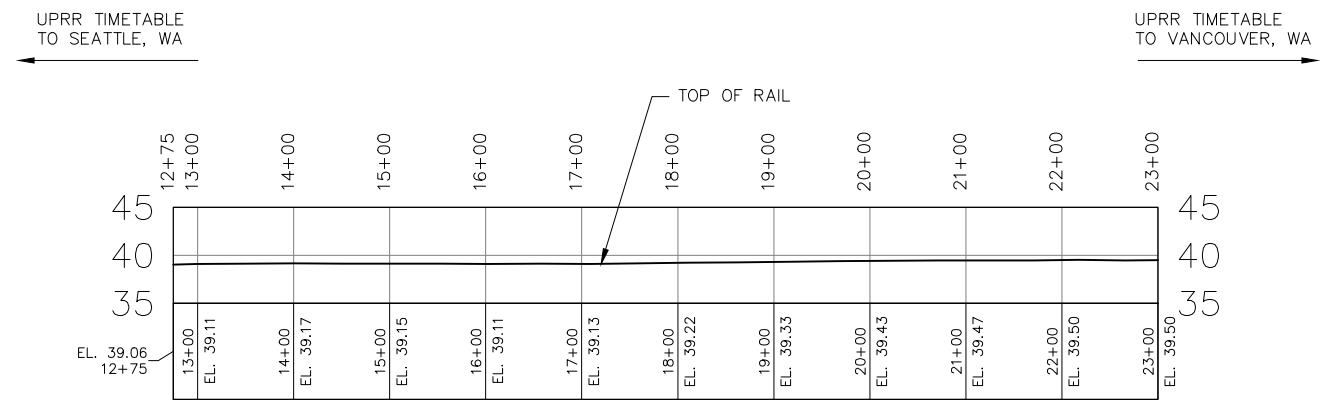
NOTE:
1. PIPE TO BE ABANDONED AND FILLED WITH SAND.



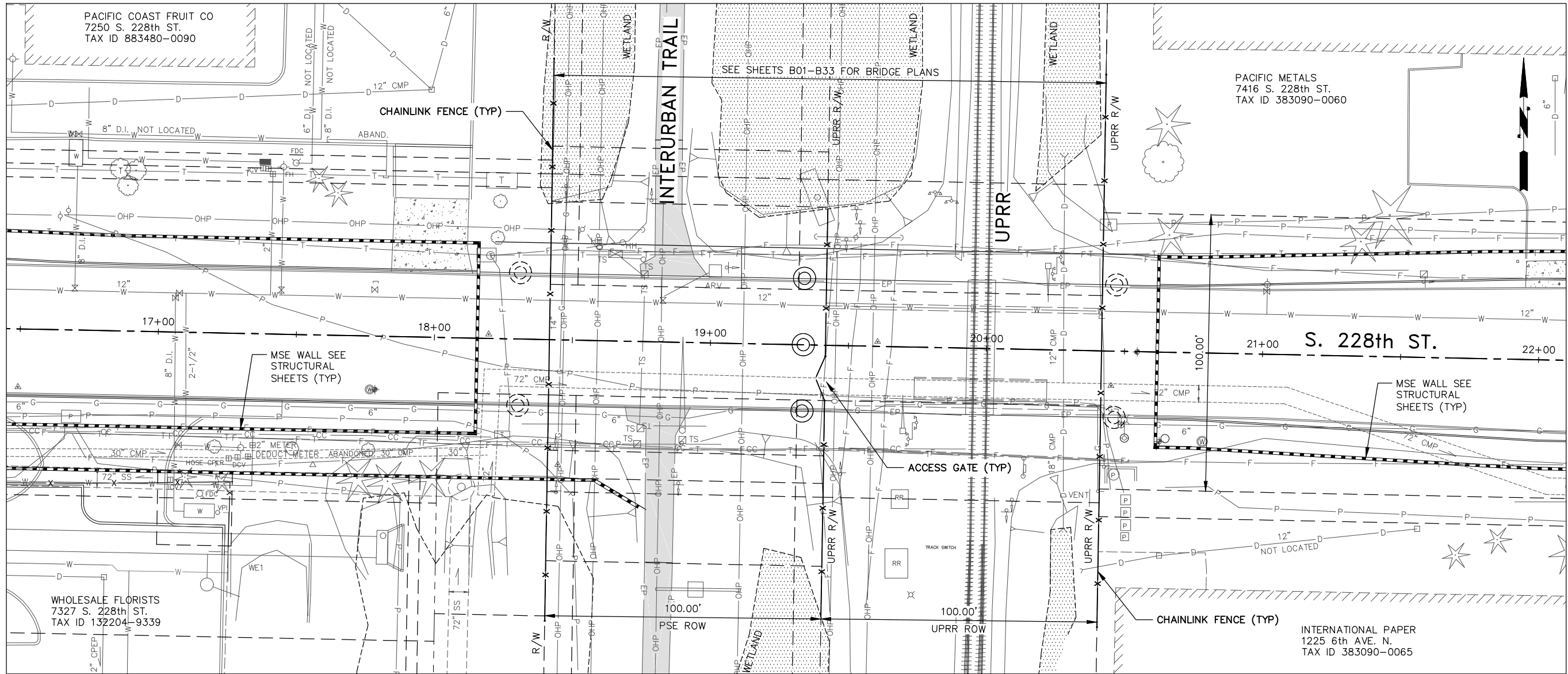
EXISTING MAIN LINE TRACK

NOTES:

1. TRACKS ARE STATIONED INDIVIDUALLY BASED ON
2. ELEVATIONS '*' ARE AT THE TOP OF THE HIGH RAIL BELOW THE BRIDGE
3. SURVEY ELEVATIONS ARE FROM CITY OF KENT SURVEY DATED 3-29-16



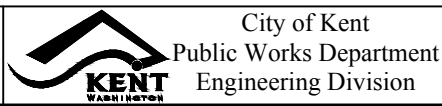
EXISTING SIDING TRACK



NO.	REVISION	BY	DATE

DESIGNED: RO	CHECKED: _____	KENT PROJECT NO. 07-3022	SCALE: 1"=20'
DRAWN: RO	PROJECT ENGR: _____	CONST. MGMT. REVIEW	HORIZ: 1"=20'
APPROVED: _____	CITY ENGINEER	DATE	VERT: -

BAR IS ONE INCH ON ORIGINAL DRAWING ADJUST SCALES ACCORDINGLY



FENCING PLAN

S. 228th STREET
GRADE SEPARATION
AT THE UNION PACIFIC RAILROAD

SHEET 6 OF 6
FILE NO.

