



WASHINGTON UTILITIES AND TRANSPORTATION COMMISSION

City of Grandview

Petitioner,

vs.

Central Washington Railroad Company (CWA)

Respondent 1

BNSF Railway

Respondent 2


DOCKET NO. TR-

PETITION TO MODIFY WARNING DEVICES AT A HIGHWAY-RAILROAD GRADE CROSSING AND REQUESTING DISBURSEMENT OF FUNDS FROM THE GRADE CROSSING PROTECTIVE FUND

USDOT Crossing No. 098614H

By filing this petition with the Washington Utilities and Transportation Commission, the Petitioner alleges that public safety requires the modification of highway-rail grade crossing warning devices under RCW 81.53.261, and requests disbursement of funds from the Grade Crossing Protective Fund.

Section 1 – Petitioner’s Information

City of Grandview
Petitioner:

Signature:
207 W. Second Street
Street Address:
Grandview, WA 98930
City, State, and Zip Code:
Mailing Address, if different than the street address:
Shane Fisher
Contact Person Name:
(509)882-9206, sfisher@grandview.wa.us
Contact Phone Number and Email:

Section 2 – Respondent’s Information

Central Washington Railroad Company (CWA)
Respondent 1:
111 University Parkway, Suite 200
Street Address:
Yakima, WA 98901
City, State, and Zip Code:
Mailing Address, if different than the street address:
Tim Marshall, General Manager
Contact Person Name:
(509)969-1746, tmarshall@cbr.com
Contact Phone Number and Email:

BNSF Railway
Respondent 2:
1310 W 11th St
Street Address:
Vancouver, WA 98660
City, State, and Zip Code:
Mailing Address, if different than the street address:
Kyle Leatham, Manager Public Projects
Contact Person Name:
(425)210-8084, Kyle.Leatham@bnsf.com
Contact Phone Number and Email:

Section 3 – Crossing Location

1. Highway/roadway:	CITY ST/Division Street		
2. Existing railroad:	Central Washington Railroad		
3. USDOT Crossing No.:	098614H		
4. GPS location:	46.251314,-119.901986		
5. Railroad mile post (nearest tenth):	0046.48		
6. City:	Grandview	County:	Yakima

Section 4 – Highway Information

1. Name of Roadway/highway:	CITY ST/Division Street		
2. Road authority:	City of Grandview		
3. Average annual daily traffic (AADT):	4780	AADT year:	2017
4. Number of lanes:	3		
5. Roadway speed:	25		
6. Is the crossing part of an established truck route?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	
7. If so, trucks are what percentage of total daily traffic?	<input type="text"/> %		
8. Is the crossing part of an established school bus route?	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	
9. If so, how many school buses travel over the crossing each day?	<input type="text"/> 2		

10. Describe any changes to the information in 1 through 9, above, expected within ten years:

No changes anticipated to the information in 1 through 9 above.

11. What is the sight distance from the stop bar (or 25 feet from the tracks if no stop bar) on both approaches to the crossing?

Sight distance from the north, for southbound traffic, is at least 400 feet. However, the controlled intersection on W 4th Street, located 300 feet north, has a sight distance less than 400 feet. Sight distance from the south, for northbound traffic, is at least 400 feet on three of the four legs of the intersection.

12. If the sight distance is less than 400 feet, describe the structures, roadway or track curvature, visual obstacles or other characteristics that limit sight distance.

Sight distance from the south, for northbound traffic on S. Division Street, is less than 400 feet due to the proximity to the controlled intersection of Division Street and W 5th Street. Obstructions for vehicles include a large building and the irregular geometry of the intersection. Additionally, the obstruction from 4th Street is a building, and there are large structures from the nearby fruit processing plant that further limit sight distance.

Section 5 -Railroad Information

1. Railroad company:

2. Type of railroad at crossing: Common Carrier Logging Industrial
 Passenger Excursion

3. Type of tracks at crossing: Mainline Siding or Spur

4. Number of tracks at crossing:

5. Average daily train traffic, freight:

Authorized freight train speed: Operated freight train speed:

6. Average daily train traffic, passenger:

Authorized passenger train speed: Operated passenger train speed:

7. Describe any changes to the information in 1 through 6 above, expected within ten years.

No changes anticipated to the information in 1 through 6 above.

Section 6 – Current Warning Devices

Indicate the type of warning devices currently located at the crossing (vehicle and pedestrian), including signs, gates, lights, train detection circuitry, and any other warning devices.

List the Advanced Warning Signs (W10 Series)

- | | | |
|---------------------------------------------------|--------------------------------------------------------------|----------------------------------------------------------|
| <input checked="" type="checkbox"/> Stop Lines | <input checked="" type="checkbox"/> Crossbucks (R15-1) | <input type="checkbox"/> Median Barriers |
| <input type="checkbox"/> Power-Off Indicator | <input checked="" type="checkbox"/> Road Markings | <input checked="" type="checkbox"/> Crossbuck Assemblies |
| <input checked="" type="checkbox"/> Warning Bells | <input type="checkbox"/> Emergency Notification System Signs | |
| <input checked="" type="checkbox"/> Cantilevers | <input checked="" type="checkbox"/> Gates | <input type="checkbox"/> Four-Quadrant Gates |

Number Flashing Light Pairs Incandescent LED

Train Detection Type:

Other:

Traffic Signal Preemption

Are the railroad signals currently interconnected with a traffic signal(s)?

- Yes No

Will this project interconnect railroad signals with the traffic signal(s) or modify the existing traffic signal preemption timing?

- Yes No

If yes, attach documentation supporting the proposed traffic signal preemption timing calculations (e.g., [TXDOT Guide for Determining Time Requirements for Traffic Signal Preemption at Highway Rail Grade Crossings](#) or similar preemption worksheet/plan), which must be certified by a professional engineer.

Section 7 – Description of Proposed Changes

Describe in detail the number and type of proposed automatic signals (vehicle and pedestrian), gates, other warning devices, and/or changes to train detection circuitry. (RCW 81.53.271) Please describe any other proposed changes at the crossing, including changes to the crossing surface, signage, pavement markings, etc. If sidewalks are being installed, please provide information on who will maintain them. Attach additional information sheets, if needed.

Advanced Warning Signs (W-10 Series)

Road Markings

Stop Lines

Gates, Vehicle and/or Pedestrian

Crossbucks (R15-1)

Crossbuck Assemblies

Median Barriers

Emergency Notification System Signs

Bells

Cantilever Lights

Number of Flashing Light Pairs

Upgrade Warning Lights to LEDs

Replace Batteries or Chargers

Upgrade Train Detection Technology

Changes to Traffic Light Interconnection/Preemption None

Other:

Will the project include installation of or modifications to sidewalks?

If yes, please describe:

No, the sidewalk improvements were completed under UTC Petition Docket TR-230370.

Will the project include changes to the crossing surface?

If yes, please describe:

No, the crossing improvements were completed under UTC Petition Docket TR-230370.

Additional information about proposed changes:

The proposed modification involves relocating the Cantilever/Gate on Division Street to comply with Federal Regulation (CFR Part 236).

Section 8 – Illustration of Crossing

Attach a detailed diagram, design drawing, map, or other illustration showing the current and proposed layout of the road, crossing surface, and railway in the vicinity of the crossing, including shoulders, sidewalks, lanes of travel, bike lanes, warning devices, pavement markings and any other applicable crossing conditions.

Section 9 – Description of Public Safety Need

Describe and support the public safety need for the proposed changes. (RCW 81.53.261)

The proposed changes to the Cantilever/Gate at Division Street are critical for public safety since current gate arms unable to adequately cover the lanes, increasing the risk of accidents. Relocating the Cantilever/Gate ensures compliance with CFR Part 236, effectively covering all lanes and

Does the project support under-resourced communities and/or rural areas? Yes No

If yes, please describe.

Section 10 – Approximate Cost of Installation and Related Work

1. Provide the approximate cost of the installation and related work for the proposed changes to signals and/or warning devices.

The cost to relocate the cantilever and crossing arm is \$25,409.00.

2. Provide an itemized breakdown of materials, names of the parties contributing to the project, including labor, and the amount each is contributing.

Install Cost = 23,526.85
Sales Tax 8% = 1,882.15

Total Cost = \$25,409.00

3. Provide the amount requested from the GCPF grant program. (RCW 81.53.281)

\$25,409.00

Section 11 – Approximate Cost of Annual Maintenance

Provide the approximate cost of annual maintenance for the signals and/or warning devices. (RCW 81.53.271)

Typically, there are no charges for the annual maintenance of the signals and/or warning devices; costs are only incurred for necessary repairs.

Section 12 – Project Completion Date



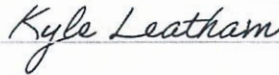
What is the estimated timeline for project completion?

The relocation was completed June 29-30, 2024.

Section 13 – Cost Apportionment

If the commission directs the installation of or changes to the warning devices requested in this petition, it will apportion installation and maintenance cost in accordance with the applicable statutes. (RCW 81.53.261-295)

Interested parties may instead enter into an agreement providing for the installation of signals or other warning devices or for the apportionment of the cost of installation and maintenance. (RCW 81.53.261) **If the parties to this petition have reached an agreement related to apportionment of costs, please sign here to confirm:**

Petitioner:  Respondent 1:  Respondent 2: 

Section 14 – Waiver of Hearing by Respondent(s)

Waiver of Hearing

The undersigned represents the Respondent(s) in the petition to modify highway-rail grade crossing warning devices at the following crossing.

USDOT Crossing No.:

We have investigated the conditions at the crossing. We are satisfied the conditions are the same as described by the Petitioner in this docket. We agree the warning devices should be modified and consent to a decision by the commission without a hearing.

If traffic signal preemption is proposed or modified with this project:
We have reviewed and have no objection to the proposed traffic signal preemption timing calculations as submitted with this petition.

Dated at , Washington, on the 1 day of January 2024.

Printed Name of Respondent 1:

Signature of the Respondent's Representative:

Title:

Phone Number:

Email:

Mailing Address:

Printed Name of Respondent 2:

Signature of the Respondent's Representative:

Title:

Phone Number:

Email:

Mailing Address:

Checklist prior to submitting petition:

- ✓ Ensure all petition fields are completed.
- ✓ Ensure parties sign Section 13 regarding any Cost Apportionment agreement, if applicable.
- ✓ Obtain signature on Waiver of Hearing (Section 14). *If respondent(s) fail to sign Waiver, advise UTC staff upon submission.*
- ✓ Attach copies of:
 - Illustration of crossing (described in section 8)
 - Proposed traffic signal preemption timing calculations, if applicable (described in section 6), and identification or documentation that the calculations are certified by a professional engineer.
 - Any other relevant documents to support the petition, including but not limited to support of public need, project information, etc.

Submitting the Application

After completing the application, file the signed application at [EFile](#). Under “Filing Type,” select “Application for Funding.”

Assistance

For questions or assistance, please contact the following UTC staff:

Mike Turcott at (360) 664-1119 or mike.turcott@utc.wa.gov

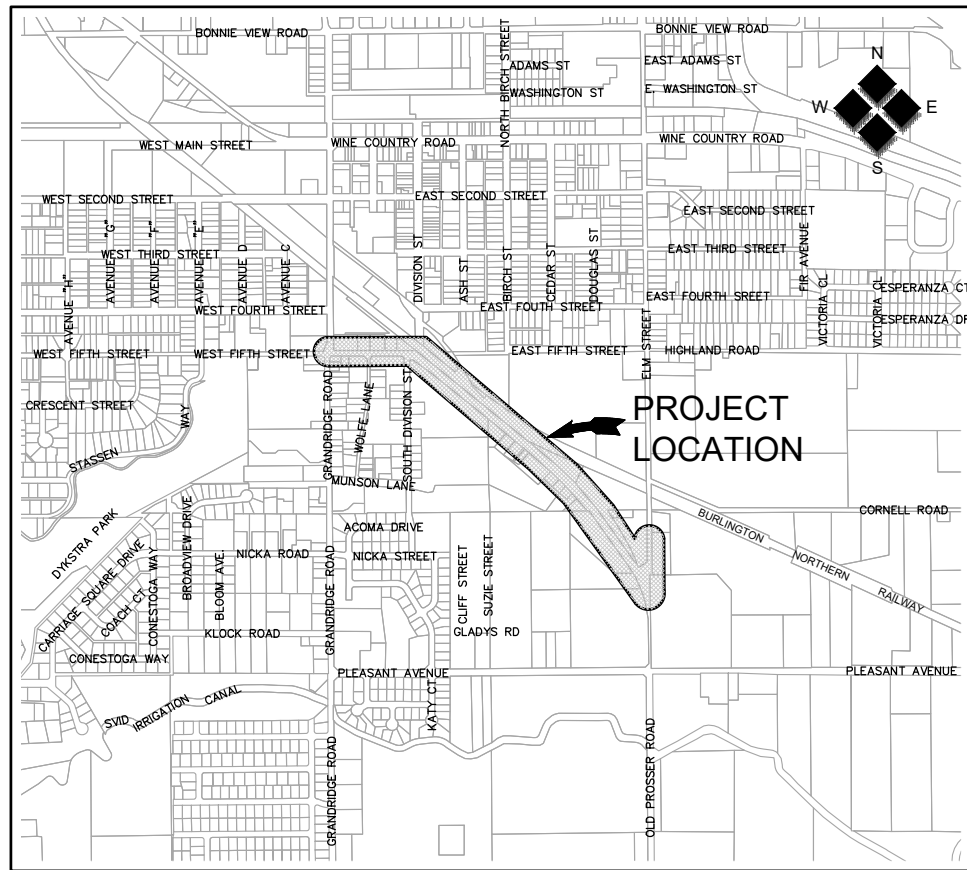
Tyler Whitcomb at (564) 669-0943 or tyler.whitcomb@utc.wa.gov

BEFORE



AFTER





VICINITY MAP
NOT TO SCALE

CITY OF GRANDVIEW

YAKIMA COUNTY

WASHINGTON


OLD INLAND EMPIRE HIGHWAY IMPROVEMENTS

FEDERAL AID NO. STPUS 8052-(003)
HLA PROJECT NO. 13134

MARCH 2023

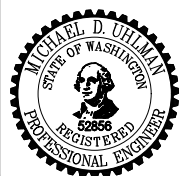
SHEET INDEX

SHEET 1	COVER SHEET
SHEET 2	LEGEND AND GENERAL NOTES
SHEETS 3-5	TYPICAL ROADWAY SECTIONS
SHEETS 6-7	TEMPORARY EROSION AND SEDIMENT CONTROL PLAN
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SHEET 33	ILLUMINATION PLAN
SHEETS 34-35	DRAINAGE STRUCTURE NOTES AND DETAILS
SHEETS 36-37	PROJECT DETAILS

 **DATUM ELEVATION**
REBAR WITH RED CAP STAMPED 'HLA CONTROL'
LOCATED AT THE WEST SIDE OF THE INTERSECTION
OF 4TH STREET AND DIVISION STREET.



2803 River Road
Yakima, WA 98902
509.966.7000
Fax 509.965.3800
www.hlacivil.com



FEDERAL AID NO. STPUS 8052-(003)		JOB NUMBER: 13134	DATE: 03-22-23
		FILE NAMES: DRAWING: Sheets-ROAD PLAN: 13134	
		DESIGNED BY: MDU	
		ENTERED BY: MEM/OCK	
REVISION		DATE	

CITY OF GRANDVIEW
OLD INLAND EMPIRE
HIGHWAY IMPROVEMENTS

COVER SHEET

SHEET
1
OF
37

LEGEND	
	EXISTING RIGHT-OF-WAY
	EXISTING CENTERLINE
	EXISTING EASEMENT
	EXISTING SECTION LINE
	EXISTING FENCE
	EXISTING WATER
	EXISTING SEWER
	EXISTING STORM DRAINAGE
	EXISTING NATURAL GAS
	EXISTING CULVERT
	EXISTING OVERHEAD POWER
	EXISTING OVERHEAD TELEPHONE
	EXISTING UNDERGROUND POWER
	EXISTING TELEPHONE
	EXISTING FIBER OPTIC
	EXISTING CABLE TV
	EXISTING ELECTRICAL
	EXISTING BLACK WATER
	EXISTING CHLORINE
	EXISTING DIGESTED SLUDGE
	EXISTING EFFLUENT
	EXISTING IRRIGATION
	EXISTING FORCE MAIN
	EXISTING GRAY WATER
	EXISTING HOT WATER SUPPLY
	EXISTING INDUSTRIAL WASTE
	EXISTING INFLUENT
	EXISTING GUY LINE
	EXISTING RAW SEWER
	EXISTING SLUDGE
	EXISTING TREATED BLACK WATER
	EXISTING TREATED WATER
	EXISTING UNDER DRAIN
	EXISTING VENT
	EXISTING WASTE ACTIVATED SLUDGE
	EXISTING SEDIMENT FENCE
	EXISTING DECIDUOUS TREE
	EXISTING EVERGREEN TREE
	EXISTING SHRUB
	EXISTING STUMP
	ASPHALT AREAS
	CONCRETE AREAS

	EXISTING MONUMENT FOUND
	EXISTING PROPERTY CORNER FOUND
	5/8" REBAR WITH CAP #33132 SET
	EXISTING WATERMAIN BLOWOFF
	EXISTING IRRIGATION VALVE
	EXISTING FIRE HYDRANT
	EXISTING PIV
	EXISTING FDC
	EXISTING WATER VALVE
	EXISTING WATER METER
	EXISTING WATER MANHOLE
	EXISTING SANITARY SEWER MANHOLE
	EXISTING CLEANOUT
	EXISTING STORM DRAIN DRYWELL
	EXISTING CATCH BASIN
	EXISTING STORM DRAIN MANHOLE
	EXISTING TELEPHONE MANHOLE
	EXISTING POWER MANHOLE
	EXISTING IRRIGATION MANHOLE
	EXISTING GAS METER
	EXISTING GAS VALVE
	EXISTING SIGN
	EXISTING MARKER SIGN
	EXISTING ANCHOR POLE
	EXISTING PULL BOX
	EXISTING TELEPHONE PED
	EXISTING UTILITY POLE
	EXISTING LIGHT
	EXISTING PEDESTRIAN/YARD LIGHT
	EXISTING TRAFFIC SIGNAL
	EXISTING PUSH BUTTON SIGNAL
	EXISTING WOOD POST
	EXISTING STEEL POST
	EXISTING HOSE BIB
	EXISTING SPRINKLER
	EXISTING STAND PIPE
	EXISTING WELL
	EXISTING MAIL BOX
	EXISTING FLAG POLE
	EXISTING WIND MACHINE
	NEW HMA
	NEW CEMENT CONCRETE SIDEWALK
	NEW PEDESTRIAN CURB RAMP
	NEW CURB AND GUTTER
	NEW SEWER LINE
	NEW WATER LINE
	NEW STORM DRAIN LINE
	NEW ELECTRICAL LINE
	NEW GAS LINE
	NEW AND/OR RELOCATED CHAIN LINK FENCE
	NEW SAWCUT LINE
	NEW MONUMENT
	NEW MANHOLE/CATCH BASIN TYPE 2
	NEW CATCH BASIN TYPE 1
	NEW DRYWELL
	NEW SEWER CLEANOUT
	NEW JUNCTION BOX
	NEW SERVICE BOX
	NEW STREET LIGHT
	NEW PEDESTRIAN LIGHT
	NEW UTILITY POLE
	NEW FIRE HYDRANT
	NEW FIRE DEPARTMENT CONNECTION
	NEW WATER VALVE
	NEW BLOW OFF VALVE
	NEW REDUCER
	NEW THRUST BLOCK
	NEW WATER METER
	NEW GAS METER
	NEW BOLLARD
	NEW MAILBOX
	NEW SIGN
	NEW RADIUS POINT
	ELEVATION AT TOP BACK OF CURB
	ELEVATION AT FLOWLINE OR SURFACE

GENERAL SITE NOTES:

- THE CONTRACTOR IS ADVISED THAT THE LOCATION AND OR ELEVATIONS OF EXISTING UTILITIES SHOWN ON THESE DRAWINGS ARE BASED UPON UTILITY INFORMATION OF RECORD AND UTILITY COMPANY MARKINGS IN THE FIELD. ALL UTILITIES TO BE RELOCATED WILL BE RELOCATED BY OTHERS UNLESS OTHERWISE NOTED.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY PERTINENT LOCATIONS AND ELEVATIONS OF UTILITY CONNECTION POINTS AND UTILITY CROSSINGS. FIELD VERIFY DEPTHS OF UTILITIES BY POTHOLING PRIOR TO BEGINNING ANY NEW CONSTRUCTION TO ALLOW FOR ADJUSTMENT IN GRADE OR ALIGNMENT. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR POTHOILING OR ADJUSTMENT OF PIPE GRADES. NOTIFY THE ENGINEER IF CONDITIONS ARE OTHER THAN DEPICTED.
- THE CONTRACTOR SHALL REMOVE ALL DEBRIS FROM THE SITE. NO BURNING WILL BE ALLOWED. THE CONTRACTOR SHALL BE REQUIRED TO SECURE AND OPERATE HIS OWN WASTE DISPOSAL SITE AT HIS OWN EXPENSE FOR THE DISPOSAL OF ALL UNSUITABLE MATERIAL, ASPHALT, CONCRETE, DEBRIS, WASTE MATERIAL, AND ANY OTHER OBJECTIONABLE MATERIAL WHICH IS DIRECTED TO WASTE. THE CONTRACTOR SHALL COMPLY WITH THE STATE OF WASHINGTON REGULATIONS REGARDING DISPOSAL OF WASTE MATERIAL AS OUTLINED IN WAC 173-304, SUBCHAPTER 461.
- AT ALL TIMES DURING CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTROLLING ON-SITE EROSION DUE TO WIND AND RUNOFF.
- THE CONTRACTOR SHALL COORDINATE AND COOPERATE WITH THE CONTRACTING AGENCY AND ALL UTILITY COMPANIES INVOLVED WITH REGARD TO RELOCATIONS OR ADJUSTMENTS OF EXISTING UTILITIES DURING CONSTRUCTION AND TO ASSURE THAT THE WORK IS ACCOMPLISHED IN A TIMELY FASHION AND WITH A MINIMUM DISRUPTION OF SERVICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING ALL PARTIES AFFECTED BY ANY DISRUPTION OF ANY UTILITY SERVICE.
- CROSS SLOPE ON ALL SIDEWALKS SHALL BE 1.5% UNLESS OTHERWISE NOTED.
- NEW ASPHALT DRIVEWAYS SHALL BE 0.17' THICK (COMPACTED DEPTH) AND SHALL BE PAID FOR AS "HMA CL 1/2-INCH PG 64-28", PER TON.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLIANCE WITH THE REGULATIONS OF THE YAKIMA COUNTY CLEAN AIR AUTHORITY. A METHOD OF DUST CONTROL DURING CONSTRUCTION SHALL BE SUBMITTED TO AND APPROVED BY THE YAKIMA COUNTY CLEAN AIR AUTHORITY. A WRITTEN COPY OF THEIR APPROVAL SHALL BE PROVIDED TO THE DEVELOPER. THE CONTRACTOR SHALL DESIGNATE A PROJECT COORDINATOR FOR CONTACT DURING CONSTRUCTION REGARDING ALLEGED AIR QUALITY VIOLATIONS AND OTHER COMPLAINTS.

GENERAL DEMOLITION NOTES:

- THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING ALL EXISTING UTILITIES WHICH ARE TO REMAIN IN SERVICE, INCLUDING BUT NOT LIMITED TO, GAS, ELECTRIC, AND TELEPHONE LINES, CABLE TV, AND WATER, SANITARY, AND STORM SEWER LINES.
- ALL DEMOLITION WORK SHALL BE PERFORMED IN ACCORDANCE WITH ALL STATE AND LOCAL CODES AND REQUIREMENTS. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR COMPLIANCE WITH ALL REQUIREMENTS INCLUDING, BUT NOT LIMITED TO, HAZARDOUS MATERIALS, DISPOSAL, AND HOURS OF OPERATION.
- ALL EXISTING BUSHES, TREES, AND TREE ROOTS SHALL BE REMOVED FROM THE PROJECT LIMITS UNLESS OTHERWISE NOTED.
- ALL EROSION CONTROL MEASURES DESIGNATED FOR THE SITE PERIMETER OR TO PROTECT EXISTING IMPROVEMENTS SHALL BE IN PLACE PRIOR TO THE START OF ANY DEMOLITION ACTIVITIES.
- ANY UNFORESEEN CONDITIONS WHICH MAY BE ENCOUNTERED OR UNCOVERED DURING THE PROCESS OF DEMOLITION SHALL BE BROUGHT TO THE ATTENTION OF THE OWNER IMMEDIATELY. ADDITIONAL EXPENSES INCURRED BY UNFORESEEN CONDITIONS SHALL BE APPROVED BY THE OWNER PRIOR TO ANY ADDITIONAL WORK BEING PERFORMED.
- THE CONTRACTOR SHALL USE WASTE SITE PROVIDED BY THE CITY OF GRANDVIEW FOR ALL SUITABLE EXCAVATED NATIVE SOIL AND GRAVEL BASE MATERIAL. THE SITE IS WITHIN TWO MILES OF THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE APPROPRIATE DISPOSAL OF ALL DEBRIS. BURNING ON SITE SHALL NOT BE PERMITTED.
- THE CONTRACTOR SHALL MOUND OVER EXISTING UTILITIES AND OTHER FEATURES DURING EARTHWORK ACTIVITIES. ANY DAMAGE TO PUBLIC UTILITIES OR ADJACENT PROPERTIES AS A RESULT OF THE DEMOLITION ACTIVITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. REPAIRS SHALL BE MADE IN A TIMELY MANNER TO THE SATISFACTION OF THE DAMAGED PARTY.
- THE CONTRACTOR SHALL NOT DISTURB EXISTING SURVEY MONUMENTS. ALL DAMAGE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

GENERAL PROJECT NOTES:

- ALL CONSTRUCTION SHALL CONFORM TO THE LATEST EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION AS PUBLISHED BY THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION (WSDOT) AND WASHINGTON STATE CHAPTER OF THE AMERICAN PUBLIC WORKS ASSOCIATION (APWA) AND THE SPECIAL PROVISIONS OF THE CITY OF UNION GAP. ALL WORK WITHIN PUBLIC RIGHT-OF-WAY OR EASEMENTS SHALL BE INSPECTED AND APPROVED BY THE CITY OF UNION GAP.
- EXCAVATION OF MATERIAL OF WHATEVER NATURE ENCOUNTERED AND DEWATERING FOR ALL UNDERGROUND ITEMS SHALL BE INCIDENTAL TO AND INCLUDED IN THE BID ITEMS. NO SEPARATE PAYMENT FOR ROCK EXCAVATION OR DEWATERING SHALL BE MADE.
- THE CONTRACTOR MUST CALL THE LOCAL UTILITY COORDINATING COUNCIL AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST FIELD LOCATIONS OF UTILITIES. LOCATION OF ALL UNDERGROUND UTILITIES SHOWN HEREON ARE APPROXIMATE AND ARE BASED ON FIELD LOCATIONS OF VISIBLE STRUCTURES SUCH AS CATCH BASINS, MANHOLES, WATER GATES, ETC., AND COMPILING INFORMATION FROM PLANS SUPPLIED BY VARIOUS UTILITY COMPANIES. ALL CONTRACTORS SHOULD CALL 1-800-424-5555 PRIOR TO ANY EXCAVATION WORK.
- EXISTING TRAFFIC SIGNS SHALL BE RELOCATED AND MAINTAINED UNTIL NEW PERMANENT SIGNING IS INSTALLED. THE EXISTING SIGNS WILL THEN BE REMOVED BY THE CONTRACTOR AND SALVAGED TO THE CITY.
- ANY DAMAGE TO PUBLIC UTILITIES OR ADJACENT PROPERTIES AS A RESULT OF THE CONSTRUCTION ACTIVITIES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE MADE IN A TIMELY MANNER TO THE SATISFACTION OF THE DAMAGED PARTY.
- THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE APPROVED PLANS, ONE (1) COPY OF THE APPROPRIATE STANDARDS AND SPECIFICATIONS, AND A COPY OF ANY PERMITS AND EXTENSION AGREEMENTS NEEDED FOR THE JOB, ON-SITE AT ALL TIMES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RECORDING AS-BUILT INFORMATION ON A SET OF RECORD DRAWINGS KEPT AT THE CONSTRUCTION SITE, AND AVAILABLE TO THE INSPECTOR AT ALL TIMES. THE CONTRACTOR SHALL DELIVER THESE DRAWINGS TO THE ENGINEER AT THE COMPLETION OF THE WORK.
- IF WORKERS ENTER ANY TRENCH OR OTHER EXCAVATION FOUR FEET OR MORE IN DEPTH THAT DOES NOT MEET THE OPEN PIT REQUIREMENTS OF WSDOT/APWA SECTION 2-09.3(3)B, IT SHALL BE SHORED AND CRIBBED. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR WORKER SAFETY AND THE ENGINEER ASSUMES NO RESPONSIBILITY. ALL TRENCH SAFETY SYSTEMS SHALL MEET THE REQUIREMENTS OF THE WASHINGTON INDUSTRIAL SAFETY AND HEALTH ACT, CHAPTER 49.17 RCW.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ANY AND ALL TRAFFIC CONTROL DEVICES AS MAY BE REQUIRED BY THE CONSTRUCTION ACTIVITIES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ROADWAYS FREE AND CLEAR OF ALL CONSTRUCTION DEBRIS AND DIRT TRACKED FROM THE SITE.
- THE CONTRACTOR SHALL MAINTAIN TWO-WAY TRAFFIC ALONG THE PROJECT LENGTH.
- THE CONTRACTOR SHALL MAINTAIN VEHICULAR AND PEDESTRIAN ACCESS TO BUSINESSES AT ALL TIMES THAT BUSINESSES ARE OPEN, UNLESS WORK IS OCCURRING IMMEDIATELY IN FRONT OF THE DOORWAY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO MAINTAIN PEDESTRIAN TRAFFIC AND BUSINESS ACCESS THROUGHOUT THE DURATION OF THE PROJECT. AT A MINIMUM, THE CONTRACTOR SHALL:
 - MINIMIZE THE DISRUPTION IN FRONT OF THE BUSINESS ACCESS BY REMOVING SIDEWALK ON EITHER SIDE OF THE ACCESS AND LEAVING THE EXISTING SIDEWALK IN PLACE AS LONG AS POSSIBLE AND LIKEWISE SHALL SEQUENCE THE INSTALLATION OF THE NEW SIDEWALK TO PROVIDE ACCESS TO THE BUSINESS;
 - PROVIDE GRAVEL SURFACING (CRUSHED SURFACING TOP COURSE) ACCESS ACROSS THE CONSTRUCTION AREA TO THE DOOR OF THE BUSINESS;
 - PROVIDE BOARDWALKS AND BRIDGING WHERE GRAVEL SURFACING CANNOT BE PROVIDED OR, BY THE NATURE OF THE BUSINESS OR WHERE DIRECTED BY THE ENGINEER, WHEELED ACCESS FOR ADA ACCESSIBILITY AND STROLLERS IS CRITICAL TO THE BUSINESS AND CANNOT BE PROVIDED THROUGH THE GRAVEL SURFACING;
 - PROVIDE TEMPORARY SIDEWALK SIGNS DIRECTING PEDESTRIANS THROUGH THE CONSTRUCTION, NOTIFYING PEDESTRIANS OF ALTERNATIVE ROUTES, AND DIRECTING PEDESTRIANS TO BUSINESSES WHERE MEANS OF ACCESS IS NOT OBVIOUS;
 - ADJUSTING TIMES OF CONSTRUCTION IMMEDIATELY IN FRONT OF A BUSINESS ACCESS TO TIMES OF THE DAY WHEN THE BUSINESS IS CLOSED, OR BUSINESS ACTIVITY IS LIGHT. FOR EXAMPLE, CONSTRUCTION IN FRONT OF A DELI WOULD BE RESTRICTED DURING THE LUNCH HOUR; AND
 - WHEN CONSTRUCTION ACTIVITIES WILL AFFECT INGRESS AND EGRESS TO A PROPERTY ALONG THE PROJECT ALIGNMENT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING THE OCCUPANT/OCCUPANTS OF THE PROPERTY 24 HOURS PRIOR TO THE CONSTRUCTION ACTIVITY BEGINNING. IF PERSONAL CONTACT WITH THE OCCUPANT IS NOT POSSIBLE, THE CONTRACTOR SHALL LEAVE WRITTEN NOTIFICATION.
- IF ANY EXISTING STRUCTURES TO REMAIN ARE DAMAGED DURING CONSTRUCTION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR AND/OR REPLACE THE EXISTING STRUCTURE AS NECESSARY TO RETURN IT TO EXISTING CONDITIONS OR BETTER.
- PACIFIC POWER AND LIGHT COMPANY (PP&L) AND CENTURYLINK MAY BE WORKING DURING THE CONTRACTOR'S WORK. THE CONTRACTOR SHALL COOPERATE WITH PP&L AND ANY OTHER UTILITIES PERFORMING WORK WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL COORDINATE WITH PP&L AND BE RESPONSIBLE FOR ANY COSTS ASSOCIATED WITH STABILIZING POLES, ANCHORS, OR OTHER FACILITIES DURING CONSTRUCTION.

HLA
Engineering and Land Surveying, Inc.

2803 River Road
Yakima, WA 98902
509.966.7000
Fax 509.965.3800
www.hlacivil.com

MICHAEL D. UHLMAN
STATE OF WASHINGTON
52856
REGISTERED PROFESSIONAL ENGINEER

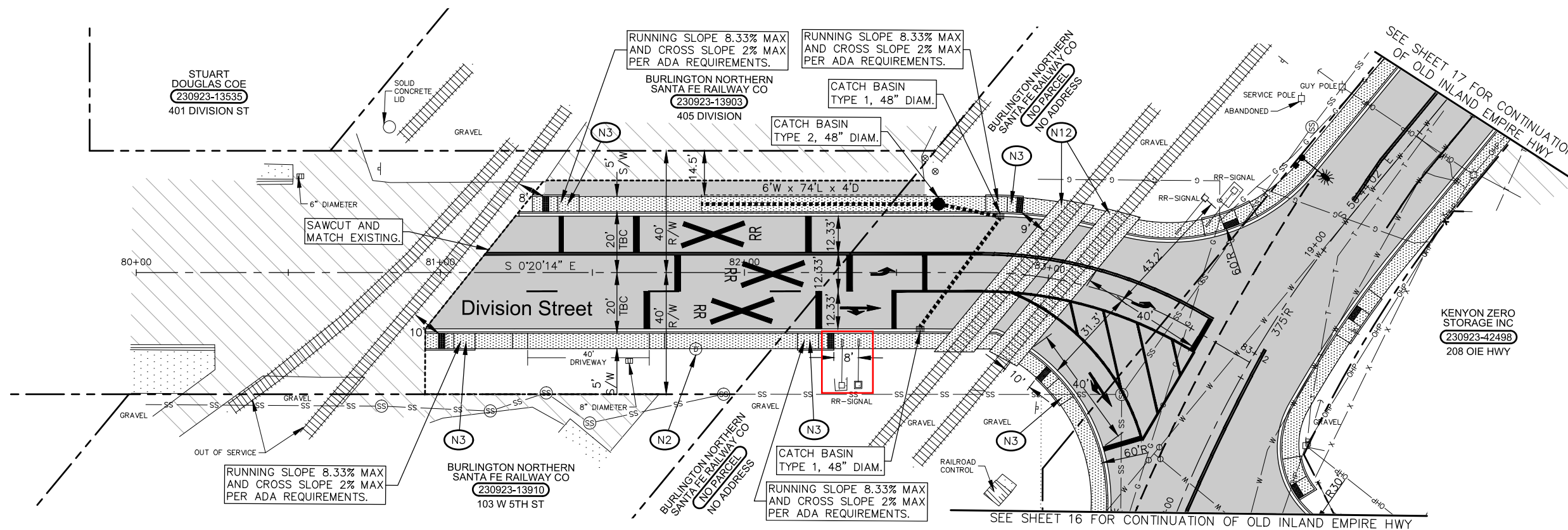


FEDERAL AID NO. STPUS 8052-(003)		JOB NUMBER: 13134	DATE: 03-22-23
REVISION		FILE NAMES: DRAWING: Sheets-ROAD PLAN: 13134	DESIGNED BY: MDU ENTERED BY: MEM/OCK

CITY OF GRANDVIEW
OLD INLAND EMPIRE
HIGHWAY IMPROVEMENTS

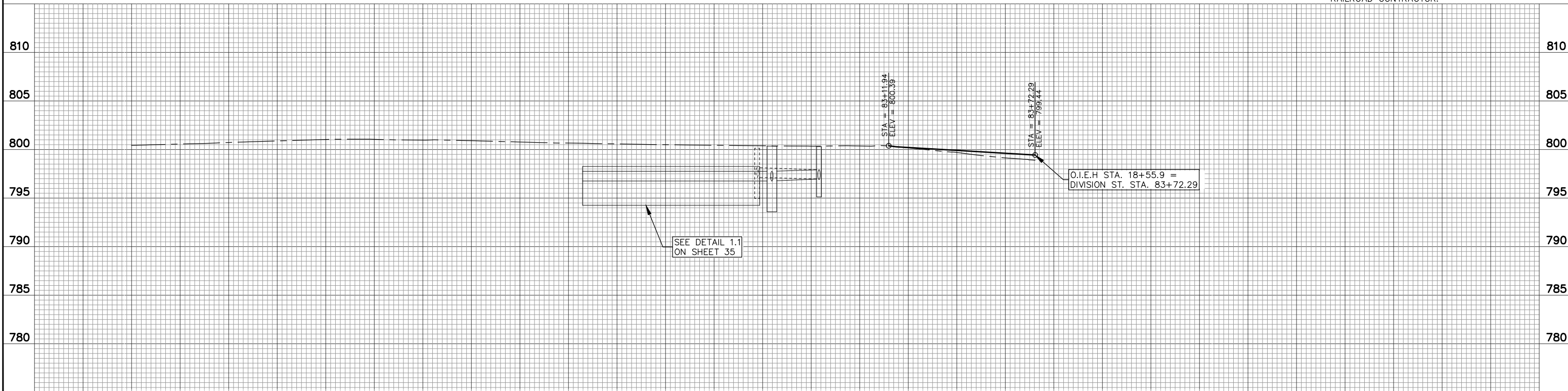
LEGEND AND GENERAL NOTES

SHEET	2
OF	37

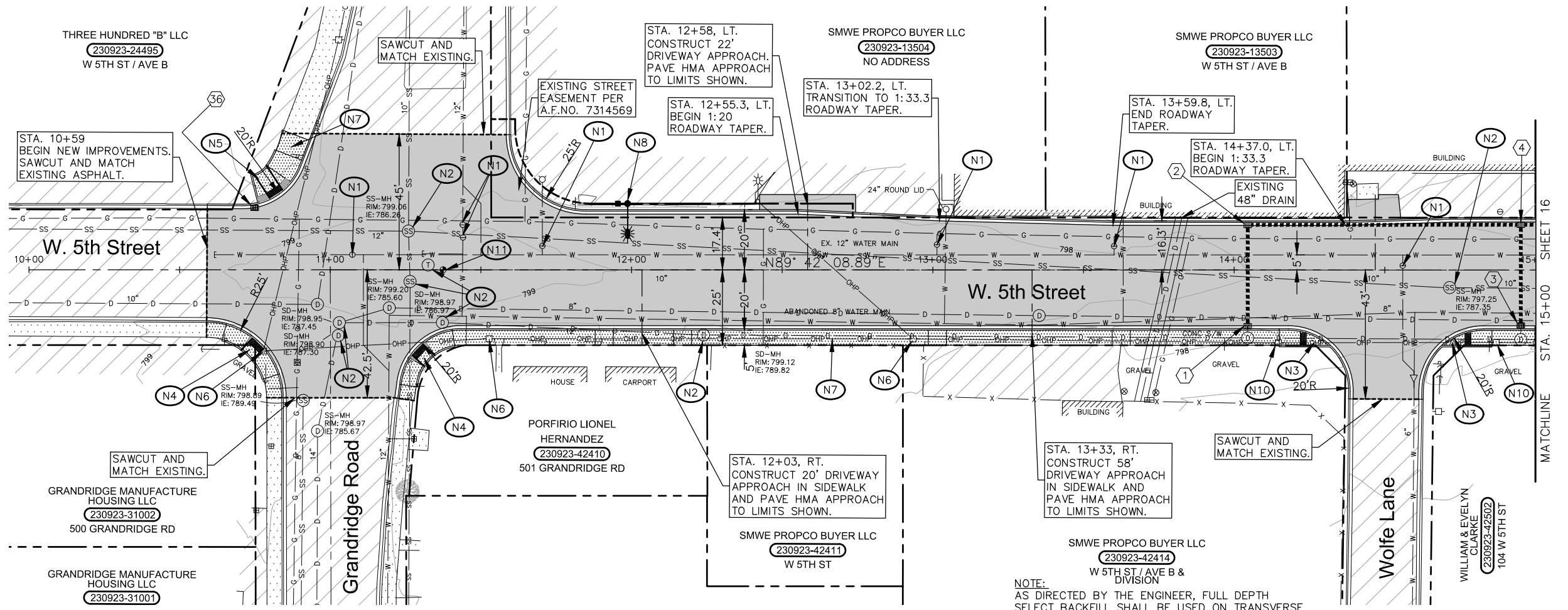
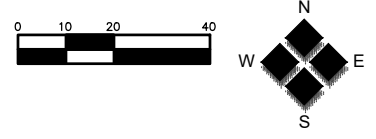


- CONSTRUCTION NOTES:**
- # DRAINAGE STRUCTURE NOTE. SEE SHEET 34.
 - N1 ADJUST VALVE BOX CASE AND COVER TO FINAL GRADE.
 - N2 ADJUST MANHOLE FRAME AND COVER TO FINAL GRADE. ROTATE CONE AS NECESSARY.
 - N3 INSTALL TYPE SINGLE DIRECTION A CURB RAMP, INCLUDING PEDESTRIAN CURB PER WSDOT STANDARD PLAN F-40.16
 - N4 INSTALL TYPE PARALLEL A CURB RAMP, INCLUDING PEDESTRIAN CURB PER WSDOT STANDARD PLAN F-40.12.
 - N5 INSTALL TYPE PARALLEL B CURB RAMP, INCLUDING PEDESTRIAN CURB PER WSDOT STANDARD PLAN F-40.12.
 - N6 UTILITY POLE/TELEPHONE PEDESTAL TO BE RELOCATED BY OTHERS DURING PROJECT.
 - N7 CONSTRUCT NEW CEMENT CONCRETE SIDEWALK 4-INCH THICK.
 - N8 INSTALL STREET LIGHT, JUNCTION BOX, CONDUIT AND CONDUCTORS PER DETAILS.
 - N9 REMOVE AND RESET FENCE TO RIGHT OF WAY LINE.
 - N10 PROTECT EXISTING UTILITY TO REMAIN.
 - N11 ADJUST MONUMENT CASE AND COVER TO FINISH GRADE. ENGINEER TO RESET MONUMENT AT CONTRACTORS EXPENSE IF DISTURBED.
 - N12 CONSTRUCT 10' WIDE CONCRETE CROSSING SURFACE PER BNSF RAILWAY ROAD CROSSING LAYOUT. WORK TO BE COMPLETED BY QUALIFIED RAILROAD CONTRACTOR.

NOTE:
AS DIRECTED BY THE ENGINEER, FULL DEPTH SELECT BACKFILL SHALL BE USED ON TRANSVERSE CROSSINGS WITHIN THE ROADWAY LIMITS.

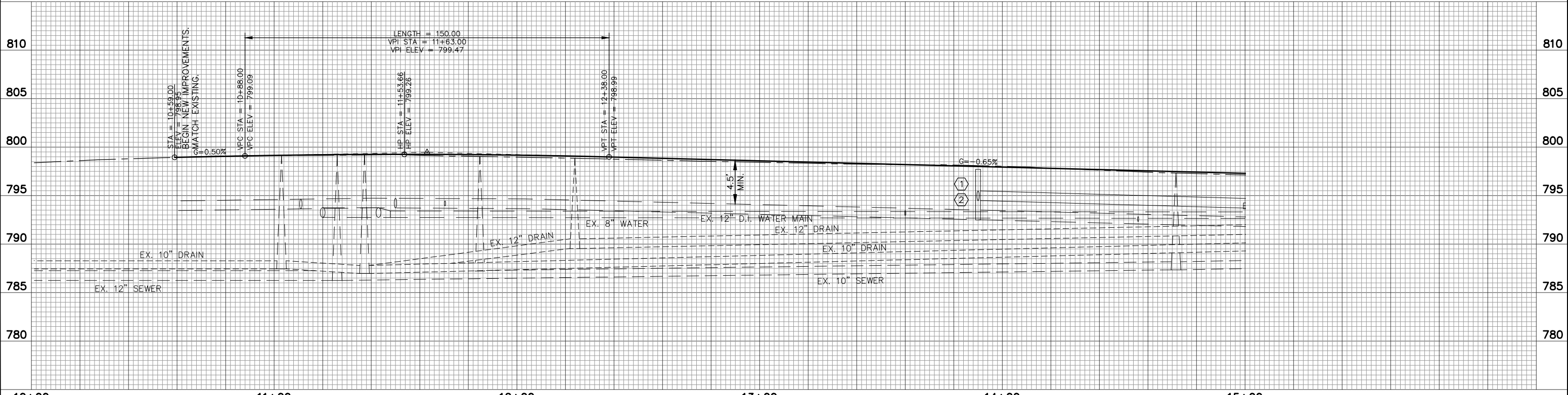


80+00	81+00	82+00	83+00	84+00	SCHEDULE A
				CITY OF GRANDVIEW OLD INLAND EMPIRE HIGHWAY IMPROVEMENTS PLAN AND PROFILE, STA. B.O.P. TO E.O.P.	
2803 River Road Yakima, WA 98902 509.966.7000 Fax 509.965.3800 www.hlacivil.com				FEDERAL AID NO. STPUS 8052-(003) JOB NUMBER: 13134 DATE: 03-22-23 FILE NAMES: DRAWING: Sheets-ROAD PLAN: 13134 DESIGNED BY: MDU ENTERED BY: MEM/OCK	
		REVISION		DATE	
					SHEET 25 OF 37

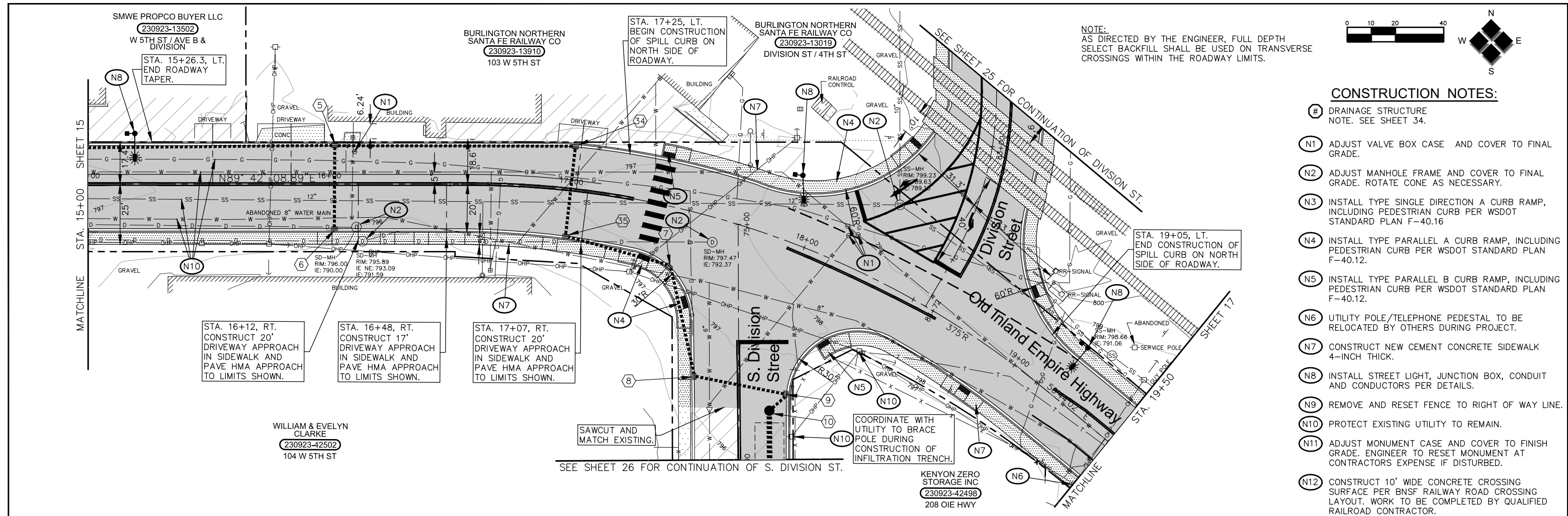


- CONSTRUCTION NOTES:**
- # DRAINAGE STRUCTURE NOTE. SEE SHEET 34.
 - N1 ADJUST VALVE BOX CASE AND COVER TO FINAL GRADE.
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 - N3 INSTALL TYPE SINGLE DIRECTION A CURB RAMP, INCLUDING PEDESTRIAN CURB PER WSDOT STANDARD PLAN F-40.16
 - N4 INSTALL TYPE PARALLEL A CURB RAMP, INCLUDING PEDESTRIAN CURB PER WSDOT STANDARD PLAN F-40.12.
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 - N10 PROTECT EXISTING UTILITY TO REMAIN.
 - N11 ADJUST MONUMENT CASE AND COVER TO FINISH GRADE. ENGINEER TO RESET MONUMENT AT CONTRACTORS EXPENSE IF DISTURBED.
 - N12 CONSTRUCT CONCRETE CROSSING SURFACE PER BNSF STANDARDS.

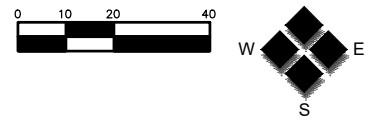
NOTE:
AS DIRECTED BY THE ENGINEER, FULL DEPTH SELECT BACKFILL SHALL BE USED ON TRANSVERSE CROSSINGS WITHIN THE ROADWAY LIMITS.



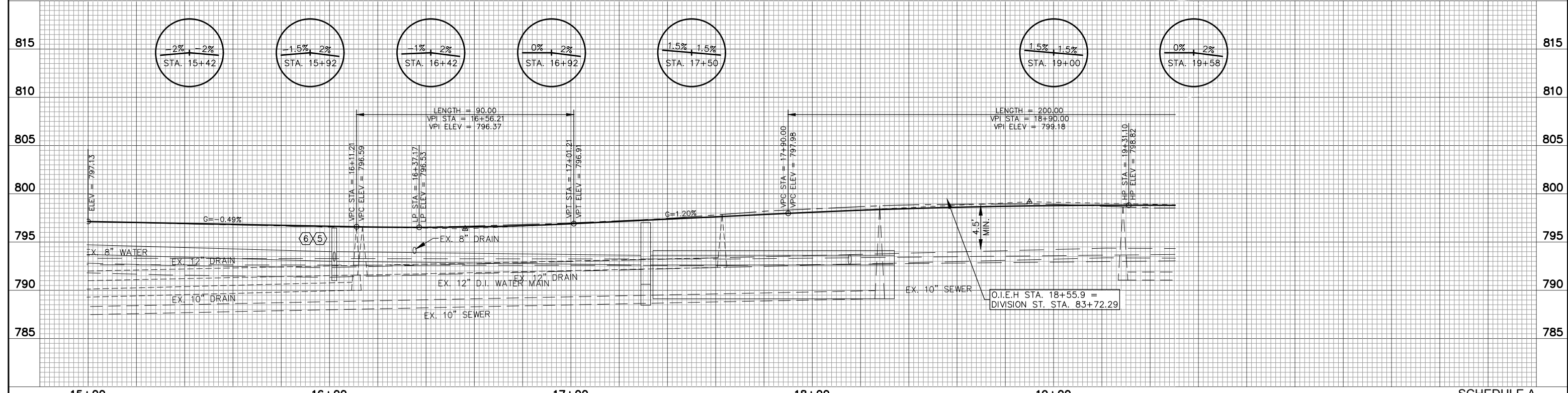
	2803 River Road Yakima, WA 98902 509.966.7000 Fax 509.965.3800 www.hlacivil.com			FEDERAL AID NO. STPUS 8052-(003)	JOB NUMBER: 13134	DATE: 03-22-23	CITY OF GRANDVIEW OLD INLAND EMPIRE HIGHWAY IMPROVEMENTS PLAN AND PROFILE, B.O.P. TO STA. 15+00	SHEET 15 OF 37
				REVISION	DATE	FILE NAMES: DRAWING: Sheets-ROAD PLAN: 13134 DESIGNED BY: MDU ENTERED BY: MEM/OCK		



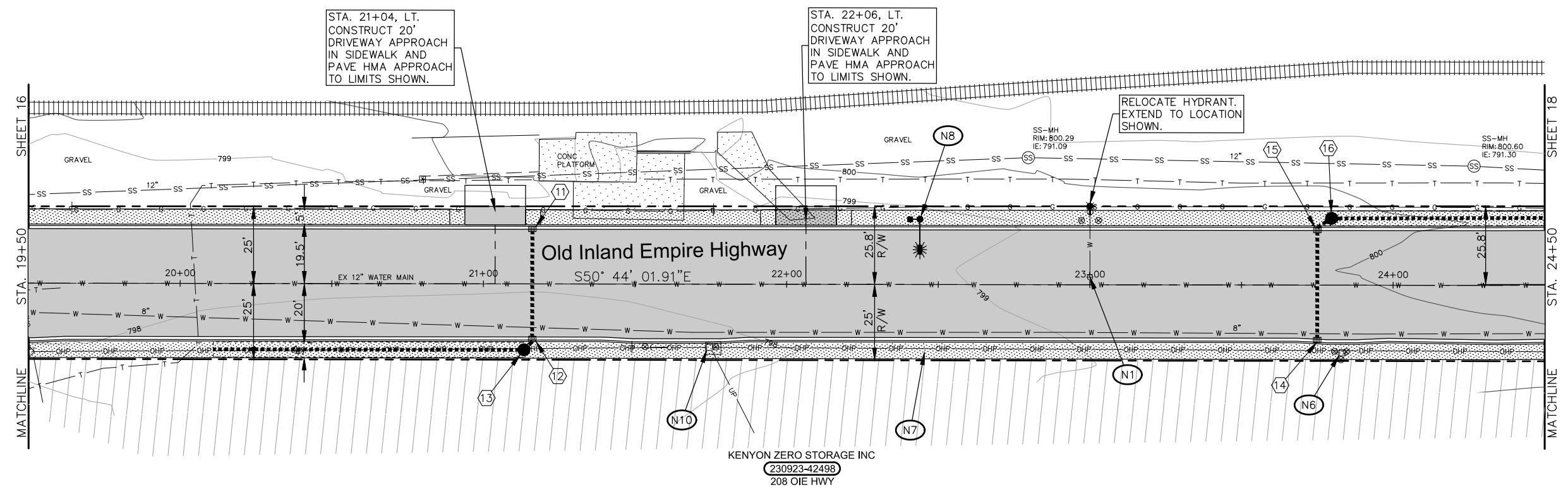
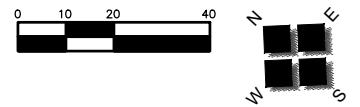
NOTE:
AS DIRECTED BY THE ENGINEER, FULL DEPTH SELECT BACKFILL SHALL BE USED ON TRANSVERSE CROSSINGS WITHIN THE ROADWAY LIMITS.



- CONSTRUCTION NOTES:**
- # DRAINAGE STRUCTURE NOTE. SEE SHEET 34.
 - N1 ADJUST VALVE BOX CASE AND COVER TO FINAL GRADE.
 - N2 ADJUST MANHOLE FRAME AND COVER TO FINAL GRADE. ROTATE CONE AS NECESSARY.
 - N3 INSTALL TYPE SINGLE DIRECTION A CURB RAMP, INCLUDING PEDESTRIAN CURB PER WSDOT STANDARD PLAN F-40.16
 - N4 INSTALL TYPE PARALLEL A CURB RAMP, INCLUDING PEDESTRIAN CURB PER WSDOT STANDARD PLAN F-40.12.
 - N5 INSTALL TYPE PARALLEL B CURB RAMP, INCLUDING PEDESTRIAN CURB PER WSDOT STANDARD PLAN F-40.12.
 - N6 UTILITY POLE/TELEPHONE PEDESTAL TO BE RELOCATED BY OTHERS DURING PROJECT.
 - N7 CONSTRUCT NEW CEMENT CONCRETE SIDEWALK 4-INCH THICK.
 - N8 INSTALL STREET LIGHT, JUNCTION BOX, CONDUIT AND CONDUCTORS PER DETAILS.
 - N9 REMOVE AND RESET FENCE TO RIGHT OF WAY LINE.
 - N10 PROTECT EXISTING UTILITY TO REMAIN.
 - N11 ADJUST MONUMENT CASE AND COVER TO FINISH GRADE. ENGINEER TO RESET MONUMENT AT CONTRACTORS EXPENSE IF DISTURBED.
 - N12 CONSTRUCT 10' WIDE CONCRETE CROSSING SURFACE PER BNSF RAILWAY ROAD CROSSING LAYOUT. WORK TO BE COMPLETED BY QUALIFIED RAILROAD CONTRACTOR.



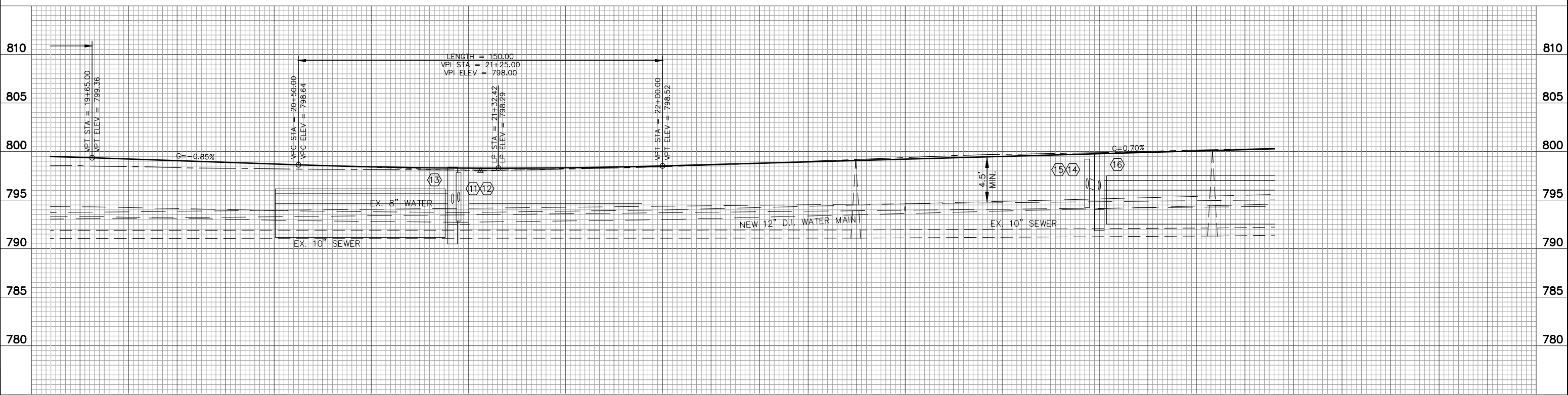
15+00	16+00	17+00	18+00	19+00	SCHEDULE A
				FEDERAL AID NO. STPUS 8052-(003) JOB NUMBER: 13134 DATE: 03-22-23 FILE NAMES: DRAWING: Sheets-ROAD PLAN: 13134 DESIGNED BY: MDU ENTERED BY: MEM/OCK	
2803 River Road Yakima, WA 98902 509.966.7000 Fax 509.965.3800 www.hlacivil.com				CITY OF GRANDVIEW OLD INLAND EMPIRE HIGHWAY IMPROVEMENTS PLAN AND PROFILE, STA. 15+00 TO 19+50	
				SHEET	16
				OF	37



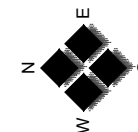
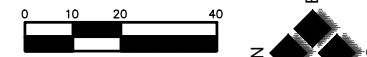
CONSTRUCTION NOTES:

- # DRAINAGE STRUCTURE
NOTE. SEE SHEET 34.
- N1 ADJUST VALVE BOX CASE AND COVER TO FINAL GRADE.
- N2 ADJUST MANHOLE FRAME AND COVER TO FINAL GRADE. ROTATE CONE AS NECESSARY.
- N3 INSTALL TYPE SINGLE DIRECTION A CURB RAMP, INCLUDING PEDESTRIAN CURB PER WSDOT STANDARD PLAN F-40.16
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NOTE:
AS DIRECTED BY THE ENGINEER, FULL DEPTH SELECT BACKFILL SHALL BE USED ON TRANSVERSE CROSSINGS WITHIN THE ROADWAY LIMITS.

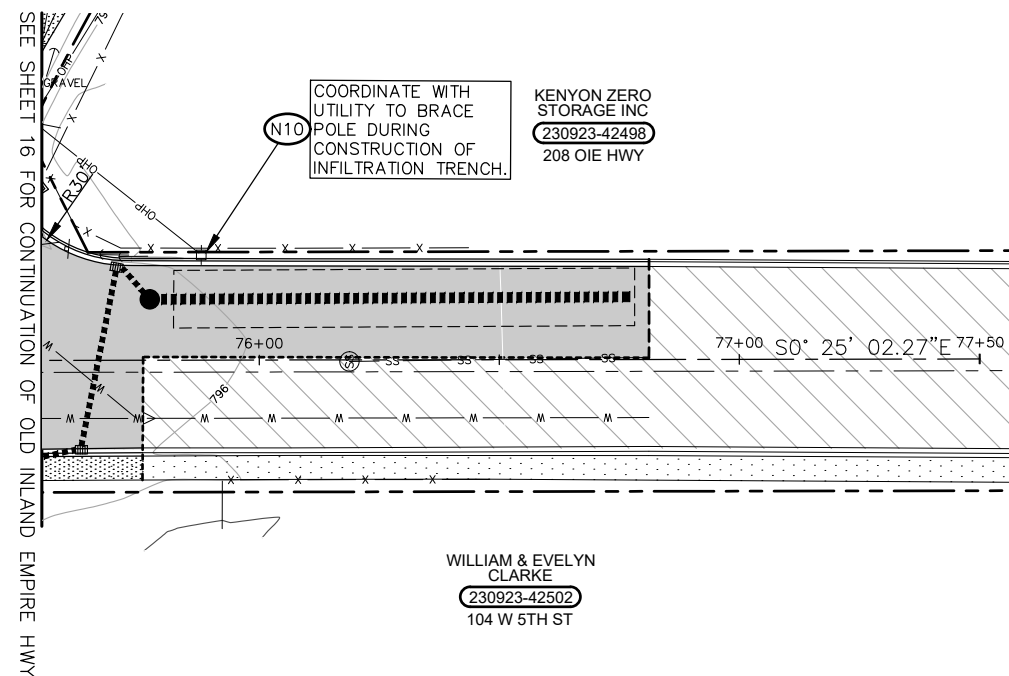


20+00	21+00	22+00	23+00	24+00	25+00
				<p style="text-align: center;">CITY OF GRANDVIEW OLD INLAND EMPIRE HIGHWAY IMPROVEMENTS</p>	
<p>2803 River Road Yakima, WA 98902 509.966.7000 Fax 509.965.3800 www.hlacivil.com</p>		<p style="text-align: center;">MICHAEL D. UHLMANN STATE OF WASHINGTON REGISTERED PROFESSIONAL ENGINEER 52656</p>		<p>JOB NUMBER: 13134 DATE: 03-22-23 FILE NAMES: DRAWING: Sheets-ROAD PLAN: 13134 DESIGNED BY: MDU ENTERED BY: MEM/OCK</p>	
		<p>FEDERAL AID NO. STPUS 8052-(003)</p>		<p style="text-align: center;">PLAN AND PROFILE, STA. 19+50 TO STA. 24+50</p>	
		<p>REVISION DATE</p>		<p style="text-align: right;">SHEET 17 OF 37</p>	

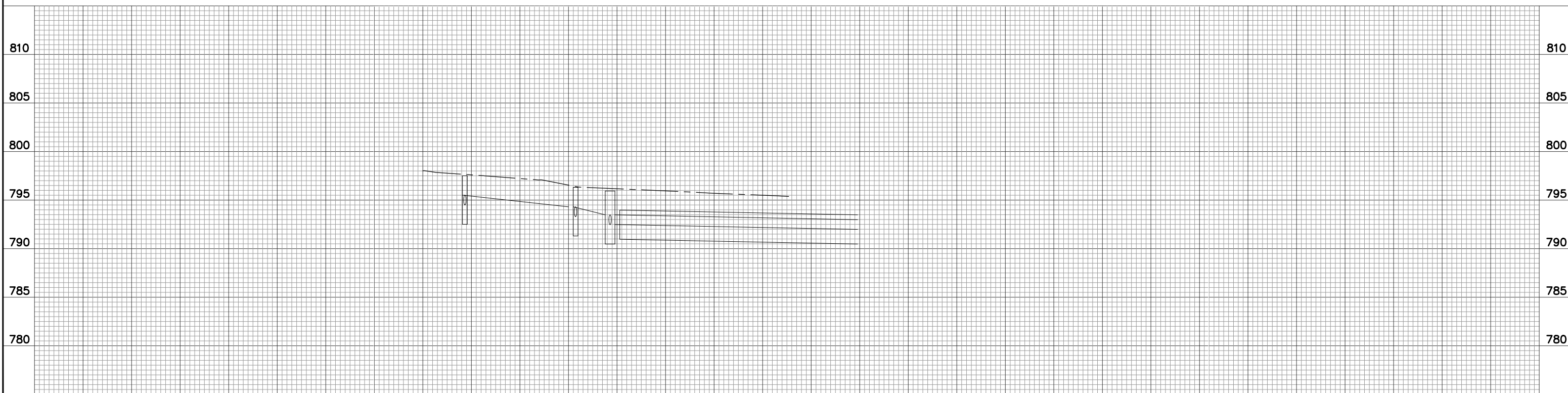


CONSTRUCTION NOTES:

- # DRAINAGE STRUCTURE
NOTE. SEE SHEET 31.
- (N1) ADJUST VALVE BOX CASE AND COVER TO FINAL GRADE.
- (N2) ADJUST MANHOLE FRAME AND COVER TO FINAL GRADE. ROTATE CONE AS NECESSARY.
- (N3) INSTALL TYPE SINGLE DIRECTION A CURB RAMP, INCLUDING PEDESTRIAN CURB PER WSDOT STANDARD PLAN F-40.16
- (N4) INSTALL TYPE PARALLEL A CURB RAMP, INCLUDING PEDESTRIAN CURB PER WSDOT STANDARD PLAN F-40.12.
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- (N7) CONSTRUCT NEW CEMENT CONCRETE SIDEWALK 4-INCH THICK.
- (N8) INSTALL STREET LIGHT, JUNCTION BOX, CONDUIT AND CONDUCTORS PER DETAILS.
- (N9) REMOVE AND RESET FENCE TO RIGHT OF WAY LINE.
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	2803 River Road Yakima, WA 98902 509.966.7000 Fax 509.965.3800 www.hlacivil.com			FEDERAL AID NO. STPUS 8052-(003)	JOB NUMBER: 13134	DATE: 03-22-23	CITY OF GRANDVIEW OLD INLAND EMPIRE HIGHWAY IMPROVEMENTS PLAN AND PROFILE, STA. B.O.P. TO E.O.P.	SHEET 26 OF 37
				REVISION	DATE	FILE NAMES: DRAWING: Sheets-ROAD PLAN: 13134		